



CITY COUNCIL WORK SESSIONS AGENDA

NRP Multifamily Development Proposal-Capital Office Park (Detailed Site Plan)
Municipal Building
25 Crescent Road
Monday, December 02, 2019
8:00 PM

Work Session Packet - *Agenda*

- *Introductions*
- *Council Discussion*
- *Questions and Answers*
- *Other Items*

NRP Multifamily Development Proposal - Capital Office Park (Detailed Site Plan)
Suggested Action: NRP Greenbelt Metro Detailed Site Plan Staff Report – 11/19/2019
Detailed Site Plans
Architectural Drawings (Building Elevations)
City Council Letter to Hewlett / Planning Board
Preliminary Plan of Subdivision Staff Report – 9/23/2019
PRAB Report – 11/20/2019

[NRP-Greenbelt-Metro-DSP-Staff-Report.pdf](#)
[Letter To Hewlett PPS - NRP Greenbelt Metro.pdf](#)
[NRP Greenbelt Metro - Staff Report - 9.23.2019 FINAL.pdf](#)
[CIVP-DSP-19014.pdf](#)
[ARCP-DSP-19014.pdf](#)
[PRAB Report 19-10 NRP Capital Office Park Detailed Site Plan.pdf](#)

City Council Work Sessions Agenda Item Report

Meeting Date: December 2, 2019

Submitted by: Judith Howerton

Submitting Department: Planning

Item Type: Work Session Item

Agenda Section:

Subject:

NRP Multifamily Development Proposal - Capital Office Park (Detailed Site Plan)

Suggested Action:

NRP Greenbelt Metro Detailed Site Plan Staff Report – 11/19/2019

Detailed Site Plans

Architectural Drawings (Building Elevations)

City Council Letter to Hewlett / Planning Board

Preliminary Plan of Subdivision Staff Report – 9/23/2019

PRAB Report – 11/20/2019

Attachments:

[NRP-Greenbelt-Metro-DSP-Staff-Report.pdf](#)

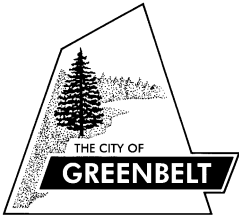
[Letter To Hewlett PPS - NRP Greenbelt Metro.pdf](#)

[NRP Greenbelt Metro - Staff Report - 9.23.2019 FINAL.pdf](#)

[CIVP-DSP-19014.pdf](#)

[ARCP-DSP-19014.pdf](#)

[PRAB Report 19-10 NRP Capital Office Park Detailed Site Plan.pdf](#)



City of Greenbelt
Department of Planning & Community Development
15 Crescent Road, Suite 200, Greenbelt, Maryland 20770
(301) 345-5417 Fax (301) 345-5418

**DETAILED SITE PLAN
GREENBELT METRO**

**STAFF REPORT
November 19, 2019**

I. GENERAL INFORMATION

Applicant/Property Owner:	The NRP Group LLC
Subject:	Detailed Site Plan
Project Name:	Greenbelt Metro (MOTIVA)
Location:	6400, 6410, and 6420 Cherrywood Lane, Greenbelt MD 20770. Located on the north side of Cherrywood Lane, approximately 800 feet west of its intersection with Kenilworth Avenue (MD 201)
Acreage:	15.89 (gross tract area) Net developable area is 10.44
Existing Zoning:	C-O (Commercial Office) /D-D-O (Development District Overlay)
Existing Land Use:	Undeveloped
Proposed Land Use:	Multi-family residential

II. BACKGROUND INFORMATION

The NRP Group LLC (NRP) is a developer, builder, and property management company headquartered in Cleveland, Ohio. In early 2018, NRP (the applicant) met with the City of Greenbelt to present a development proposal for new construction of multifamily housing in the Capital Office Park at the intersection of Cherrywood Lane and Ivy Lane next to the Federal Courthouse. The city gave positive feedback and was especially interested in an earlier version that promised a mixed use development that included a retail component. The retail component was found to not be economically feasible.

The applicant filed a Site Development Concept Plan (storm water management plan) with Prince George's County (The County) in January, 2019 that was approved end of May, 2019. The applicant immediately trailed this approval by filing a Preliminary Plan of Subdivision (PPS) application in July 2019 with the Maryland-National Capital Park and Planning Commission (M-NCPPC). The PPS was referred to the City for review and comment, upon which it went through the city's hearing process including three (3) advisory board meetings (APB, Green ACES, and PRAB), a City Council work session, and finally a City Council hearing on September 23, 2019 where it received support with conditions. The conditions are laid out in a letter from the City Council to the County Planning Board. The PPS was heard by the Prince George's County Planning Board on October 10, 2019 and has been approved with conditions.

Immediately following approval of the PPS, the city received notice that a Detailed Site Plan (DSP) was accepted by M-NCPPC. The DSP is following the same process for review and comment as the PPS, meeting with three advisory boards (APB, PRAB, and Green ACES) and the City Council for a work session and at a regular meeting.

III. PROPOSED DEVELOPMENT

The DSP proposes two 5-story apartment buildings and an amenity building. Each of the buildings will offer secure entry, elevator access (2 elevators per building), climate controlled corridors, trash and recycling rooms, resident and guest lobbies, mail rooms, and large package reception. The gross floor area of the residential buildings total 388,225 SQ FT and the amenity building totals 7,082 SQ FT.

- The apartment buildings will consist of a total of 354 residential units allocated as:

- 55-Studio
- 157-One Bedroom
- 123-Two Bedroom
- 19-Three Bedroom

- The amenity building will include:

- Leasing and administration office
- Business and conference center
- Package concierge
- A clubhouse with wellness/fitness center
- Indoor social and entertainment spaces
- Pool with deck and locker rooms
- Outdoor grilling stations, fire pit and social areas

- Other amenities and features planned are:

Dog Park
Dog Spa (adjacent to the Dog Park) / Dog Wash (equipment only)
Community Gardens
A Tot lot (Play area)
Bike racks (indoor secured bicycle storage and outdoor racks)
Bike share (Future Capital Bike share station)

The Site plan proposes two points of vehicular access from Cherrywood Lane. The main entrance aligns with Ivy Lane and the second is approximately 200ft west of the main entrance, further south on Cherrywood Lane. A total of 454 parking spaces are proposed via surface parking lots located behind the multifamily buildings. The parking spaces will include 36 individual garage units. Nine (9) accessible parking spaces are provided (distributed between lots) along with 4 more in the individual garage units, 2 in each garage. Interior bicycle parking is planned for 80 bicycles - all in one building, on the east, while 36 are planned for the exterior to be at different locations throughout the site providing a total of 136 bicycle parking spaces. There will be 6 electric vehicle charging locations also distributed in the different parking lots. In addition to the 6 charging stations there will be provision for charging ports within the individual garages.

The applicant is proposing a Capital Bikeshare station installation on site. Coordination with the County's Department of Public Works and Transportation (DPW & T) will be required. The applicant is proposing bus stop improvements for the bus stop located in immediate proximity to the project, including installation of a bus shelter designed to the new city standards.

Architectural features will include curated corner elements and building facades will contain a combination of sustainable material such as brick masonry, cementitious siding, cementitious panels, metal and glass. Signage will include one monument sign at the proposed main entrance and a second sign - a mounted blade sign - is proposed to the west of the main entrance, between the access drives.

The streetscape is to be improved with landscaping and a sidewalk. The applicant will be required to install a 5ft sidewalk along its (Cherrywood Lane) frontage. Staff plans to work with applicant on being in conformance with the City's approved Complete and Green Streets policy and on ensuring the implementation of the improvements identified in the *Preliminary Concept Design Report: Cherrywood Lane Complete and Green Street Project* along the applicant's frontage (only required to address half width of Cherrywood Lane).

IV. ANALYSIS

A. Zoning

The project area is zoned Commercial-Office (C-O) with a Development District Overlay Zone (DDOZ). The project is required to conform to the requirements of the *2013 Approved Greenbelt Metro Area and MD 193 Corridor Plan and Sectional Map*

Amendment (Sector Plan). This zoning/Sector Plan provides for a ‘dwelling, multi-family’ use. The community consists of commercial office buildings and a federal courthouse. The project site is surrounded by: United States Courthouse zoned O-S (open-space) to the northeast, vacant land zoned R-R (rural residential) to the northwest, and Capital Office Park zoned C-O across Cherrywood Lane.

B. ENVIRONMENT AND LANDSCAPING

Storm water Management (SWM)

The project site has an existing drainage pond that will remain. The pond was designed to treat storm water for the existing Capital Office Park located on the south side of Cherrywood Lane and does not have enough capacity to handle the run-off from the proposed development. At the PPS stage, the applicant provided a study and exhibits to demonstrate their findings that retrofitting the existing SWM facility would require significantly more space with greater impacts compared to providing a second SWM facility thus necessitating 2 ponds. A new storm water management (detention) pond has been proposed to attenuate a 100-year storm event.

In addition, the DSP shows utilization of micro bio-retention filtration systems throughout the development site (around 30 in total). The county has required these for this project. The Stormwater Management Act of 2007 calls for implementing environmentally sensitive design (ESD) to the maximum extent practicable. These provide curbside storm water management benefits by integrating site design, natural hydrology and smaller controls to capture and treat runoff. They also minimize impervious surfaces.

Due to the nature of the soil conditions on this site, the applicant has stated that pervious paving materials are not an option and is therefore proposing asphalt (bituminous) for the parking lot.

Sustainability

As set forth in the sector plan, Leadership in Energy and Environmental Design (LEED) practices are desired for new development. While the applicant does not intend to pursue LEED certification for this project, some of the features will meet these standards and there are aspects of the project that are specifically environmentally friendly and energy conserving such as:

- The lighting possibly up to 100 percent will use high performance or LED lighting systems.
- Low flow plumbing fixtures
- Interior recycling chutes on every floor
- Apartments will be sub-metered for electricity and water consumption and will also include:
 - Programmable thermostats
 - High-efficiency HVAC equipment
 - Energy star lighting

- Energy star appliance packages
- Vehicle charging stations (as well as charging ports within the individual garages)
- There will be micro bio-retention systems throughout the development (up to 30)
- Proposed community gardens

It should be noted that the presence of air conditioning condensers on the roofs is limiting any potential alternative use (e.g. placement of solar panels).

Woodlands

The site is subject to the provisions of the *Woodland and Wildlife Habitat Conservation Ordinance* and development activities must be in conformance with an approved Type 2 Tree Conservation Plan (TCP II). The applicant has prepared a TCP II – indicating how conservation requirements will be met. This project requires 3.04 acres of off-site woodland conservation. The City has advocated for tree mitigation to occur within the watershed being impacted, preferably within the City. The applicant is willing to work with the County and City to find an appropriate bank with available acreage to meet woodland mitigation requirements.

It should be noted that the last eligible woodland mitigation site in the City was subject to an approved Detailed Site Plan for the vacant parcel adjacent to Capitol Cadillac. Staff is presently working to determine the current status of this mitigation site in light of a new DSP being prepared for the Capitol Cadillac site. If this site is available, it should provide a little less than 2 acres of the required acreage. The remaining amount would have to be found in an appropriate bank via the County.

Noise

The site is located in the vicinity of Edmonston Road and is adjacent to I-95/495 Capital Beltway both of which are roadways that are regulated for noise. The applicant submitted a phase I noise analysis study during the PPS review which concluded that, for both buildings in certain areas, transportation noise levels may exceed acceptable levels, per County and State requirements. The applicant has submitted a Phase II noise study with the DSP. The analysis is based upon site information including existing and proposed topography, existing roadway alignments, projected roadway traffic volumes, the proposed building layout, and architectural plans.

The report indicates - by showing noise measurements - exact building locations that will be impacted by noise levels greater than transportation noise policy guidelines allow. The report also specifies the mitigation measures necessary to maintain noise levels that comply with noise regulations. This would include specific building construction materials such as upgraded windows and doors which meet specific STC ratings¹ which must be installed for select residential units in both buildings. If using these specified materials, the interior noise levels would comply with the County's residential interior noise regulations.

¹ The STC rating is a number value which describes a building element's (wall, window, door, roof etc) ability to reduce noise transmission from one side of the partition to the other.
STC - Sound Transmission Class

Landscaping

The sector plan generally defers the landscaping category to, and therefore DSP should meet the requirements of the *2010 Prince George's County Landscape Manual* (Landscape Manual). The Landscape Plan proposes all native species for plantings in all categories (shade, ornamental, evergreen, and shrubs) which are distributed along the edges of the site, throughout the parking lot and around the club house/amenity building.

The project site fronts a public street, Cherrywood Lane, and no new public streets are proposed. The proposed buildings along Cherrywood Lane are separated from public right-of-way by a change of grade and retaining walls. This resulting streetscape is proposed to be improved with landscaping and a sidewalk. The DSP shows air conditioning condenser units in front of both buildings along Cherrywood Lane. The applicant responds that there are no other locations to place them as there is not sufficient space on the roof to accommodate all equipment. Staff has requested screening and revising the Landscape plan to increase plantings/buffer.

To the north, the DSP is proposing community gardens next to the building on the west at its northern edge, across the lot from the club house, which the sector plan encourages. There was concern as to whether that particular area would get adequate sunlight to support gardening/plants as it may be overshadowed by the height of the apartment building. The applicant is awaiting a sun study report, however, states that so far has received feedback that the area will receive adequate sunlight for the proposed activity.

There will be a small length of wall constructed at the northwest corner of the amenity building sidewalk which will be a segmental block wall. Staff has requested the applicant provide a detail for this wall along with a detail for the fence that will go around the pool. A chain link fence is proposed around the planned storm water facility. The city prefers not to use chain link fences, however, the applicant argues that it ensures safety and is best for the general welfare of citizens as it discourages persons from trespassing into the storm water management facility. If the applicant cannot switch to a black wrought iron fence, which would be more attractive, then staff is requiring they provide additional landscaping that would screen the chain link fence better than what is currently shown on the Landscape plan.

To the east, the building is set back further (than the building on the west) as the land slopes on the eastern edge. Along with a landscape buffer, the applicant is proposing a retaining wall with a 3'6" steel rail fence at the top of the slope to conceal the proposed parking lot which comes out closer to Cherrywood Lane on that end. Staff believes additional vegetative buffer is needed as the slope is steep enough that the parking lot would still remain visible from the street. The dog park and the tot lot/play area will be located on the east side of the property. Staff has requested detail for review and comment for both areas and would like to see some provision of shade either via shade trees or commercial shade structures.

The applicant has recently proposed a social space near the entrance past the monument sign to the right, a plaza like area to engage the street frontage, which will include

benches and shading and will be the site of the future Capital Bikeshare station. The space design is in development and the applicant plans to submit a complete vision to the city for review.

To the west, on both access drives, parking comes right up to the front of the property - the angled head-in parking on the west side of main entrance driveway and on both sides at the secondary access drive. Staff has requested the Applicant screen the parking area to the west so that it is not visible from Cherrywood Lane, and raised concerns about the angled parking along the main exit drive, closest to Cherrywood Lane.

C. Parking and Loading

The applicant is providing a total of 454 parking spaces consisting of 9 handicap accessible surface parking spaces, 36 garage spaces of which 4 are handicap accessible, 124 compact spaces, and 285 standard parking spaces. According to the code, up to one-third (1/3) of the required number of parking spaces in any parking lot may be compact car spaces. The Code requires that a minimum of 9 spaces shall be reserved for the physically handicapped. The breakdown is shown in the table below:

PARKING PROVIDED:			
GARAGE	ACCESSIBLE SPACES:	4 SPACES	
	GARAGE SPACES:	32 SPACES	
	TOTAL:	36 SPACES	
SURFACE	ACCESSIBLE SPACES:	9 SPACES	
	STANDARD SPACES (9'X18'):	285 SPACES	
	COMPACT SPACES (8'X16.5'):	124 SPACES	29.67%
	TOTAL:	418 SPACES	
GRAND TOTAL*:		454 SPACES	
* INCLUDES 6 ELECTRIC VEHICLE CHARGING SPACES OF WHICH 1 IS ACCESSIBLE.			

The county code would require 525 parking spaces for these (amount and type) units; however, the sector plan only allows 80 percent of that number at a maximum which is 420, putting the provided number at 34 spaces over. Staff does not have an issue with this overage because following other recent residential developments in Greenbelt West specifically Greenbelt Station in the south core; parking has been and continues to be a problem. The County reviewer has asked that staff help the applicant make a strong argument if the city wants to keep all 454 spaces. Staff is creating a list of comparisons with other developments within the city (and perhaps other nearby developments in neighboring jurisdictions) to demonstrate the need.

The sector plan requires a minimum of 1 bicycle parking space for every 2 multifamily dwelling units, which would total 177 parking spaces. The applicant is providing a total of 136. Eighty (80) of those would be interior bicycle parking all in one building (specifically the one on the east side), while 36 are planned for the exterior to be at

different locations throughout the site. Staff sees no problem and agrees with the applicant that this number of bicycle parking spaces would be sufficient.

D. Transportation

Traffic Impact Analysis

The project site is subject to a previous preliminary plan of subdivision approval that would have generated more trips than the current land use proposal and therefore the County did not require any transportation improvements. With two (2) new access drives being proposed along Cherrywood Lane staff requested to see a recent traffic study of the Cherrywood Lane and Ivy Lane intersection, as well as see impacts on Edmonston Road (MD 201) and Greenbelt Road (MD 193). The city conditioned its support for the PPS on the applicant submitting a Traffic Impact Study with its Construction in the Right-of-Way permit application.

The access permit application was submitted with a traffic impact analysis which was conducted in accordance with the adopted M-NCPPC Transportation Guidelines, and includes an unsignalized intersection analysis for the Cherrywood Lane and Ivy Lane intersection. The report found that the intersection meets the applicable unsignalized intersection analysis test/s. Staff has referred the traffic impact study to the City's traffic consultant, Greenman Pedersen, Inc (GPI), for technical review and comment.

Pedestrian and Bicycle Improvements

The applicant will be required to install a contiguous sidewalk along its frontage and staff is working with the applicant on being in conformance with the City's recently approved Complete and Green Street Policy and on ensuring the implementation of the improvements identified in the *Preliminary Concept Design Report: Cherrywood Lane Complete and Green Street Project* along the applicant's Cherrywood Lane frontage (only required to address half width of Cherrywood Lane). To this end, the applicant agrees to contribute \$100,000 towards the Cherrywood Lane project, where the timing of this contribution will be agreed upon prior to the issuance of the first building permit. The applicant is proposing bus stop improvements for the bus stop located in immediate proximity to the project, including installation of a bus shelter designed to the new city standards.

Following submission of a Bicycle and Pedestrian Impact Statement (BPIS), the resulting calculated fees shall be applied towards a Capital Bikeshare station installation on site (upon entrance of the project site to the right/east which will also serve as social space with benches and shading). Coordination with the Department of Public Works and Transportation (DPW& T) will be required. This will coincide with county's installation of the Capital Bike Share program for which Greenbelt is already marked 5 locations including the Greenbelt Metro Station.

E. Parks and Recreation

The applicant will address park and recreation needs through provision of on-site private recreational facilities. In addition, the applicant has agreed to contribute \$50,000 to the City's Recreation Department for use towards improvements at the Springhill Lake Recreation Center.

Amenities proposed include a clubhouse (amenity building) with a swimming pool, a wellness/fitness center, a tot lot (play area), a dog park; a dog spa which will be housed in the building adjacent to the dog park, and community gardens.

Staff is concerned with the tot lot (play area) being adjacent to the dog park as a review of literature on dog park planning shows that these should not be located next to each other for the reason that each of these parks/play areas require the full attention of an accompanying responsible adult. The applicant argues that they believe they are compatible uses, and due to soil condition and environmental requirements for construction is limited in terms of options as to where the play area should be located.

It is a condition of the approval of the PPS that the applicant agrees to execute a Private Recreation Facilities Agreement with the City to provide for the retention and future maintenance of proposed recreation facilities. The executed agreement will be recorded among land records of Prince George's County.

V. RECOMMENDATION

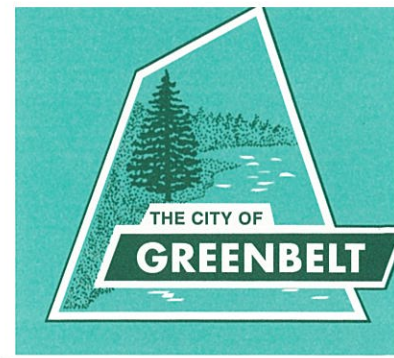
At this time staff recommends conditional support of the Detailed Site Plan for Greenbelt Metro Station (Capital Office Park). The conditions are as follows:

1. The Applicant revise the Landscape Plan to adequately screen surface parking areas from Cherrywood Lane.
2. The Applicant revise the Detailed Site Plan/Landscaping Plan to include a formal social space at the proposed bike share station location.
3. The Applicant agrees to avoid and/or limit locating air conditioning condenser units along its Cherrywood Lane frontage. Where unavoidable, the Applicant shall site the units to minimize aesthetic impacts and revise the Landscape Plan to include appropriate screening to be reviewed by the City.
4. The Applicant submit to the City details on the segmental block wall on the northwest corner (behind the pool) of project site.
5. The Applicant submit to the City a detail for the pool fence.
6. The Applicant submit a detailed list of equipment proposed for the tot lot and dog park.
7. The Applicant agrees to consider solar installation on the clubhouse.
8. The Applicant agrees to work with the city's Arts Coordinator on identifying potential opportunities for the installation of public art within the social space at the proposed bike share station.

9. The Applicant revises the Landscaping Plan to provide additional landscape screening along the southeast corner of the site.
10. The Applicant revise sign details to accurately reflect the lettering/name proposed (MOTIVA) on the signs.
11. The Applicant revise the DSP to remove the minor monument sign located at the western access drive.

CITY OF GREENBELT

25 CRESCENT ROAD, GREENBELT, MD. 20770-1886



September 24, 2019

CITY COUNCIL

Emmett V. Jordan, Mayor
Judith F. Davis, Mayor Pro Tem
Colin A. Byrd
Leta M. Mach
Silke I. Pope
Edward V.J. Putens
Rodney M. Roberts

Chairman Elizabeth M. Hewlett
Prince George's County Planning Board
14741 Governor Oden Bowie Drive
Upper Marlboro, MD 20772

RE: Preliminary Plan of Subdivision (PPS 4-19010) – NRP Greenbelt Metro

Dear Chairman Hewlett:

The Greenbelt City Council has reviewed the Preliminary Plan of Subdivision (PPS) for Greenbelt Metro, and on September 23, 2019 voted 5 to 2 to support the PPS with conditions. Attached is a list of the conditions approved by the City Council, and accepted by the applicant. The City respectfully requests that its conditions are supported by the Prince George's County Planning Board and included as conditions of approval by the Planning Board, if the Board recommends approval of the PPS.

The PPS proposes a 354 unit multi-family development on land previously approved for commercial office. The City was enthused by the applicant's earlier vision of a mixed use, multi-family development accessible to transit built at the Capital Office Park and that at the time included a retail component. The City is disappointed that the PPS under consideration does not include a retail component and has requested the applicant reconsider the introduction of a retail use as the project moves forward.

Over the last year, the Applicant has worked closely with the City to address the city's concerns relating to density, open space, transportation, recreational amenities, and the environment. The City believes that with the Applicant's agreement to the City conditions, these issues will be adequately addressed at the time of detailed site plan and looks forward to supporting this project as it moves through the development review process.

Thank you for the opportunity to review and comment. If you have any questions regarding the City's position on this matter please contact Judith Howerton, Community Planner II at (240) 542-2040.

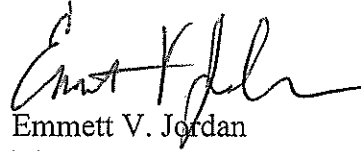
A NATIONAL HISTORIC LANDMARK

(301) 474-8000 FAX: (301) 441-8248

www.greenbeltmd.gov



Sincerely,

A handwritten signature in black ink, appearing to read "Emmett V. Jordan". The signature is fluid and cursive, with a large initial "E" and a long, sweeping underline.

Emmett V. Jordan
Mayor

cc. City Council
Honorable Todd Turner, County Council Chair
Nicole Ard, City Manager
Terri Hruby, Director of Planning and Community Development
Judith Howerton, Community Planner II
Christopher Davis, M-NCPPC, Subdivision and Zoning
Todd Pounds, City Solicitor
Matt Tedesco, McNamee Hosea

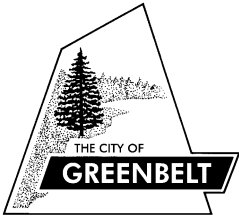
City of Greenbelt
Preliminary Plan of Subdivision (PPS 4-19010) – NRP Greenbelt Metro

Greenbelt City Council Conditions of Support
September 23, 2019

1. The applicant shall obtain a construction in the right-of-way permit from the City of Greenbelt for the construction of the proposed access drives on Cherrywood Lane, and frontage improvements prior to the issuance of the first building permit. The access permit application shall include a traffic impact study. The traffic impact study shall be conducted in accordance with the adopted M-NCPPC Transportation Guidelines, which shall also include an unsignalized intersection analysis for the Cherrywood Lane and Ivy Lane intersection. If that intersection fails the applicable unsignalized intersection analysis, the applicant shall then provide a signal warrant analysis, as required by the M-NCPPC Transportation Guidelines. The traffic impact study shall be reviewed by the City, and if traffic improvements are required as a result of the findings of the traffic impact study, said improvements shall be funded and constructed by the applicant, with timing to be agreed upon prior the issuance of the first building permit.
2. The applicant shall construct frontage improvements along Cherrywood Lane that include sidewalk, bike lane, lighting, bus stop/shelter, and street trees consistent with the City's approved Complete and Green Street Policy. In addition, the applicant agrees to contribute \$100,000.00 towards the City's Cherrywood Lane Complete and Green Street Project. Timing of said contribution shall be agreed upon prior to the issuance of the first building permit by the City.
3. The applicant agrees to provide off-site woodland mitigation/conservation in conformance with and pursuant to Subtitle 25 of the County Code and the Woodland and Wildlife Habitat Conservation Technical Manual, and agrees to work with the City to identify if there are any suitable woodland mitigation banks within the City. All costs associated with such mitigation shall be the responsibility of the Applicant.
4. Excluding non-native invasive species, the applicant agrees to mitigate the loss of trees (≥ 6 inches in caliber) in the stream buffer associated with utility installation and grading operations. At the time of Detailed Site Plan, a mitigation plan shall be submitted for review by the City of Greenbelt.
5. At the time of Detailed Site Plan, the applicant shall submit a Phase II noise study in accordance with M-NCPPC guidelines for review by the City.
6. At the time of Detailed Site Plan, the applicant shall submit a recreation package to the County and to the City of Greenbelt that includes details of the private recreational facilities in accordance with the standards outlined in the Prince Georges' County Park and Recreation Facilities Guidelines to include a pool and associated club house and amenities, pet spa, pet wash equipment, dog park, tot lot, and community garden. In

addition, the applicant agrees to contribute \$50,000.00 to the City's Parks and Recreation Department for use towards City improvements planned for the Springhill Lake Recreation Center. Timing of said contribution shall be agreed upon prior to the issuance of the first building permit by the City.

7. Prior to the submission of a final record plat, the Applicant agrees to execute a Private Recreation Facilities Agreement with the City to provide for the retention and future maintenance of proposed private recreation facilities. The executed agreement shall be recorded among the land records of Prince Georges County.



City of Greenbelt
Department of Planning & Community Development
15 Crescent Road, Suite 200, Greenbelt, Maryland 20770
(301) 345-5417 Fax (301) 345-5418

**PRELIMINARY PLAN OF SUBDIVISION
GREENBELT METRO**

**STAFF REPORT
August 23, 2019**

Revisions: 8/29/2019, 9/6/2019, 9/23/2019

I. General Information

Applicant/Property Owner:	The NRP Group LLC
Subject:	Preliminary Plan of Subdivision
Project Name:	Greenbelt Metro
Location:	6400, 6410, and 6420 Cherrywood Lane, Greenbelt MD 20770. Located on the north side of Cherrywood Lane, approximately 800 feet west of its intersection with Kenilworth Avenue (MD 201)
Acreage:	15.89 (gross tract area) Net developable area is 10.44
Existing Zoning:	C-O (Commercial Office) /D-D-O (Development District Overlay)
Existing Land Use:	Undeveloped
Proposed Land Use:	Multi-family residential

II. Background Information

The NRP Group LLC (NRP) is a developer, builder, and property management company headquartered in Cleveland, Ohio. In early 2018, NRP met with the City of Greenbelt to present a development proposal concept for new construction of multifamily housing, and a small amount of retail space in the Capital Office Park at the intersection of Cherrywood Lane and Ivy Lane. The meeting with the Greenbelt Advisory Planning

Board (APB) took place on January 18, 2018 and the City Council Work session was held on March 5, 2018.

The applicant has filed a Preliminary Plan of Subdivision (PPS) application with the Maryland-National Capital Park and Planning Commission (M-NCPPC) and the PPS is scheduled to be considered by the Prince George's County (The County) Planning Board on October 10, 2019. The PPS has been referred to the City for review and comment.

The subject properties were subject to a Preliminary Plan of Subdivision in 1994, that created the three subject lots, and a 24 acre outlot. The outlot is subject to a 100 year floodplain easement. The PPS conditionally approved the development of 556,000 gross square feet of office space.

III. Proposed Development

The PPS proposes two 5-story apartment buildings and an amenity building. The two apartment buildings will consist of 354 residential units altogether with the one, two, and three bedroom apartments allocated as 212, 123, and 19 respectively. There is no retail component associated with the current proposal, with the applicant citing it was determined not to be marketable. The on-site amenities will include a clubhouse with a swimming pool and a wellness/fitness center; a dog park; and a dog spa which will be housed in the building adjacent to the dog park. The majority of surface parking (454 spaces) provided is behind the apartment buildings however two (one level) parking garage buildings requiring paid (subscription) usage will be provided as well. These will be a total of 36 spaces, 18 in each. A total of 130 bicycle racks are proposed most of which will be indoors (in the basement level of one of the buildings). Six (6) electric vehicle charging stations will be provided, with locations to be determined at Detailed Site Plan (DSP).

IV. Analysis

A. Zoning

The project area is zoned Commercial-Office (C-O) with a Development District Overlay Zone (DDOZ). This zoning provides for multi-family housing. The project is required to conform to the requirements of the *2013 Approved Greenbelt Metro Area and MD 193 Corridor Plan and Sectional Map Amendment* (Sector Plan).

The C-O zone allows density up to 20 dwelling units (du) per net acre, however, M-NCPPC planning staff has indicated that the Greenbelt DDOZ modified the table of uses for the C-O Zone, and allows multi-family development at a density to be approved at the time of Detailed Site Plan. Staff requested clarification from M-NCPPC planning staff on its determination that the subject development is not subject to the density requirements of the C-O Zone (20 du per acre or 208 units), M-NCPPC planning staff response is that they have determined that the footnote does not apply as the use table does not

specifically reference the footnote. They have also added that this will be discussed and further explained in the technical staff report to provide clarification on this issue.

B. Environment

Primary Management Area (PMA)ⁱ

This site contains a total of 5.77 acres of PMA comprised of a 100-year floodplain, steep slopes that extend outside of the PMA, and stream buffer. The PMA is located along the northwest property line extending to approximately half of the northeast property line, covering 36.3% of the subject property. Floodplains, wetlands, streams, and steep slopes are required to be protected under the County's Subdivision Regulations.

- **Floodplain Impacts:**

The project proposes a new pond to attenuate a 100-year storm event. The proposed pond will have floodplain impacts and the County is requiring the applicant to create compensatory storage within the floodplain area to off-set the impacts. The pond will also require the removal of 1,600 SF (0.037 acres) of woodlands.

The applicant is proposing a 0.82 acre reduction of the PMA from 100-year floodplain because of the installation of the pond.

- **Stream Buffer Impacts:**

Temporary impacts are proposed to the stream buffer along the eastern property line for the installation of utilities and to grade the site such that it is viable for the overall development. The stream buffer is very lightly wooded with the vegetation occurring closer to the stream channel. Much of the vegetation is invasive species including the Bradford pears.

Less than 700SF of forest within the stream buffer is proposed to be disturbed and it is located at the head of the stream buffer. All proposed impacts are temporary for grading and utilities and on site elements are proposed to be placed within the stream buffer. The buffer will be planted and/or allowed to afforest over time.

Woodlands

The site is subject to the provisions of the Woodland Conservation Ordinance and 3.28 acres of woodland conservation is required. The applicant has prepared a Tree Conservation Plan (TCP). The applicant originally proposed fee-in-lieu for the value of 3.28 acres, however, at the Subdivision and Development Review Committee (SDRC) review session it was clarified that the County generally does not accept a fee-in-lieu (for woodland conservation requirement) in excess of 1 acre. Historically, the City has also been opposed to fee-in-lieu and has advocated for tree mitigation to occur within the

watershed being impacted, preferably within the City. The applicant is willing to work with the County and City to find an appropriate bank with available acreage to meet woodland mitigation requirements. It should be noted that the last eligible woodland mitigation site in the City was subject to an approved Detailed Site Plan for the vacant parcel adjacent to Capitol Cadillac. Staff is working to determine the current status of this mitigation site in light of a new DSP being prepared for the Capitol Cadillac site.

Noise

The site is located in the vicinity of Edmonston Road and is adjacent to I-95/495 Capital Beltway both of which are roadways that are regulated for noise. The applicant has submitted a noise study, which concludes that, for both buildings in certain areas, noise levels may exceed acceptable levels, per County and State requirements.

The units exposed will require further analysis and may require modifications to proposed standard building construction. Modifications could include increased window/door STCⁱⁱ ratings. The report also states that further analysis would be required to determine the exact mitigation designs necessary, which would be established once architectural plans (building elevations, window/door schedule, unit plans) are further developed.

The City will review and approve a noise attenuation plan and the applicant will be required to comply with Prince George's County residential noise regulations as well as the State of Maryland requirements.

Low Impact Development and Green Infrastructure

The project will be integrated with the ongoing Cherrywood Lane Complete and Green Streets retrofit project which applies low impact development strategies as well as green infrastructure. The applicant will either fund or construct Phase 2 of the project once engineered designs are completed. There may be other opportunities for green infrastructure and/or for low impact development (in addition to ESDs which are incorporated in the project, see storm water management) however, these will be evaluated more closely at the time of detailed site plans.

C. Storm water Management (SWM)

A Site Development Concept Plan was filed with Prince George's County on January 4, 2019 and approved on May 31, 2019. There is an existing SWM pond on the western corner of the tract that serves the adjacent commercial development on Ivy lane. The applicant is proposing to construct an additional SWM pond. Planning staff had concerns about this second SWM pond taking up open space where there might have been an opportunity for functional (recreational) programming. The preference would have been if the applicant could upgrade the existing pond in its obligation to meet the County's floodplain policy requirements. In response to planning staff's concern, the applicant has provided a study and exhibits to demonstrate their findings that retrofitting the existing

SWM facility will require significantly more space with greater impacts compared to providing a second SWM facility and therefore having two (2) ponds.

The Stormwater Management Act of 2007 calls for implementing environmentally sensitive design (ESD) to the maximum extent practicable. The project site will incorporate several types of ESD storm water facilities (up to 36) distributed throughout the property. These provide curbside storm water management benefits by integrating site design, natural hydrology and smaller controls to capture and treat runoff. They also minimize impervious surfaces.

D. Transportation

Traffic Impact Study

This site is the subject of a 1994 approved PPS. The same parcels J, K, and L were approved for the development of 556,000SF (~12 acres) of office/commercial development. This approval proposed an access drive off Cherrywood Lane, at its intersection with Ivy Lane. Associated with the approved PPS, was a 1992 traffic study which the County has indicated is still valid for the PPS currently under consideration.

Based upon this information, the current applicant submitted a Transportation Guidelines Checklist, which has been approved by the County, and a Memorandum (Memo). The Memo points out that the resolution for the previous PPS indicates the trip cap at 945 AM and 875 PM peak hour trips. The Memo also reports that the applicant's traffic/transportation consultant has estimated that 354 multifamily residential units would generate 184 AM and 212 PM peak hour trips, numbers well below the cap and as such determining that a full Traffic Impact Analysis (TIA) is not required and that the proposed development meets the transportation conditions of the 1994 PPS proposal without requiring additional traffic mitigation.

The city planning staff has expressed concerns with a full TIA not being submitted with this application. Those concerns include:

- Based on the 1992 TIA the Cherrywood Lane and Ivy Lane intersection at some point would not function at an acceptable level without signalization - the timing of this is unclear.
- Traffic counts/numbers in the study were based on assumptions and future projections, since then, the Greenbelt Metro station has opened and the US Federal Courthouse has been built, actual numbers could be provided.
- The 1992 report, on page 4 of the study, states that Cherrywood lane was "a newly constructed five-lane roadway from Greenbelt Road to Kenilworth Avenue." Since then, the capacity of the roadway has been reduced.
- Finally, with two new access drives being proposed off Cherrywood Lane and the modifications that have been made to Ivy Lane and Cherrywood Lane to accommodate bike lanes and medians, a traffic study/traffic signal warrant study is need to ensure safety.

Pedestrian and Bicycle Improvements

The applicant has conducted a bicycle and pedestrian analysis and submitted a Bicycle and Pedestrian Impact Statement (BPIS) to the County per guidelines. The statement shows the total cap for proposed off site bicycle and pedestrian facilities (given a cap of three hundred dollars (\$300.00) per unit of residential development proposed in the application) as:

$$\$300 \times 354 \text{ units} = \$106,200$$

The applicant is pursuing Capital Bikeshare installation on site and proposes to implement this using the above stated fee. Coordination with the Department of Public Works and Transportation (DPW& T) will be required. The applicant is proposing bus stop improvements for the bus stop located in immediate proximity to the project, including installation of a bus shelter designed to the new city standards.

The applicant will be required to install a contiguous sidewalk along its frontage and staff is working with the applicant on being in conformance with the City's recently approved Complete and Green Street Policy and on ensuring the implementation of the improvements identified in the *Preliminary Concept Design Report: Cherrywood Lane Complete and Green Street Project* along the applicant's Cherrywood Lane frontage (only required to address half width of Cherrywood Lane).

E. Parks and Recreation

The subdivision regulations of Prince George's County require that residential subdivisions address park and recreation needs through the following means:

1. Dedication of land to the appropriate public agency which can then be used by the public for parks and recreation activities. The area of land to be dedicated increases as the project density increases. Under this scenario the land area to be dedicated for the subject development would be 1.6 acres.
2. Provision of adequate private recreation facilities to service the development. There is no formula or standard which defines what is considered adequate, so this determination is subjective, and is usually negotiated between the developer and the public agency involved.
3. Payment of a fee-in-lieu of equal to five percent (5%) of the total new market value of the land as stated on the final assessment notice issued by the State Department of Assessments and Taxation. Staff is working with the City Treasurer on calculating this payment amount.
4. A combination of two or three of the above referenced options.

The subdivision regulations require that any new residential development provide sufficient recreation and open space/and or resources to satisfy the needs of the new residents of the proposed development. For the subject development, the population to be served is 354 housing units, which would result in a population of approximately 850 new residents at the rate of 2.4 persons per du. The applicant is proposing to meet its mandatory parkland dedication requirements through the provision of on-site private recreation amenities. Amenities proposed include a clubhouse with a swimming pool and a wellness/fitness center; a tot lot, a dog park; a dog spa which will be housed in the building adjacent to the dog park, and community gardens. The applicant has submitted an estimated value of the proposed on-site private recreation facilities.

As Greenbelt lies outside of the Maryland-Washington Metropolitan District, the proposed project is subject to Mandatory Parkland Dedication requirements, but staff believes it is at the discretion of the City, and not the County Planning Board. Staff is currently working with the City Treasurer and Acting Directors of the Department of Recreation to identify the city's added recreation needs as a result of the proposed development and as it relates to the applicant's requirement to meet mandatory parkland dedication requirements.

F. Schools

It is estimated that the proposed development will yield 42 elementary students, 19 middle school students and 26 high school students. As of September 30, 2017, the Prince George's County Official Enrollment report found that schools in the City of Greenbelt are over capacity. The utilization of Greenbelt schools ranges from 107% (Greenbelt Elementary) – 150% (Springhill Lake Elementary). While there is concern for the impact the proposed development will have on schools, the County's Subdivision regulations do not require a finding of adequacy for schools, and therefore a PPS cannot be denied based on school adequacy. Instead, all new residential development is required to pay a school facilities surcharge fee for every residential unit. Per County Council Resolution 55-2019 the school facilities surcharge for developments located outside the Capital Beltway is \$16,698 per unit. Therefore the subject development is required to pay a school facilities surcharge fee totaling \$4,992,702 calculated as:

$$(354 \text{ multifamily units} - 55 \text{ studio apartments}^1) \times 16,698 = \$4,992,702$$

G. Public Safety

The County Subdivision regulations require that the PPS address the issues of the adequacy of fire and rescue and police services (although only as it relates to County services). The City provides full police service to the project area. The proposed project is subject to a public safety surcharge fee. Pursuant to Section 10-192.11(2)(A) of the County code, "at least twenty five (25%) of the revenue collected from a surcharge imposed on construction that is located in a municipal corporation that maintains a police department shall be distributed to the municipal corporation's police department." The

¹ Studio apartments are not factored in school facilities surcharge calculations

public safety surcharge fee for fiscal year 2020 is \$2,630 per building permit in the developed tier, resulting in the City receiving \$232,755 towards public safety.

V. Recommendation

At this time staff recommends conditional support of the Preliminary Plan of Subdivision for Greenbelt Metro Station (Capital Office Park). The conditions are as follow:

1. The applicant shall obtain a construction in the right-of-way permit from the City of Greenbelt for the construction of the proposed access drives on Cherrywood Lane, and frontage improvements prior to the issuance of the first building permit. The access permit application shall include a traffic impact study. The traffic impact study shall be conducted in accordance with the adopted M-NCPPC Transportation Guidelines, which shall also include an unsignalized intersection analysis for the Cherrywood Lane and Ivy Lane intersection. If that intersection fails the applicable unsignalized intersection analysis, the applicant shall then provide a signal warrant analysis, as required by the M-NCPPC Transportation Guidelines. The traffic impact study shall be reviewed by the City, and if traffic improvements are required as a result of the findings of the traffic impact study, said improvements shall be funded and constructed by the applicant, with timing to be agreed upon prior the issuance of the first building permit.
2. The applicant shall construct frontage improvements along Cherrywood Lane that include sidewalk, bike lane, lighting, bus stop/shelter, and street trees consistent with the City's approved Complete and Green Street Policy. In addition, the applicant agrees to contribute \$100,000.00 towards the City's Cherrywood Lane Complete and Green Street Project. Timing of said contribution shall be agreed upon prior to the issuance of the first building permit by the City.
3. The applicant agrees to provide off-site woodland mitigation/conservation in conformance with and pursuant to Subtitle 25 of the County Code and the Woodland and Wildlife Habitat Conservation Technical Manual, and agrees to work with the City to identify if there are any suitable woodland mitigation banks within the City. All costs associated with such mitigation shall be the responsibility of the Applicant.
4. Excluding non-native invasive species, the applicant agrees to mitigate the loss of trees (≥ 6 inches in caliber) in the stream buffer associated with utility installation and grading operations. At the time of Detailed Site Plan, a mitigation plan shall be submitted for review by the City of Greenbelt.
5. At the time of Detailed Site Plan, the applicant shall submit a Phase II noise study in accordance with M-NCPPC guidelines for review by the City.
6. At the time of Detailed Site Plan, the applicant shall submit a recreation package to the County and to the City of Greenbelt that includes details of the private

recreational facilities in accordance with the standards outlined in the Prince Georges' County Park and Recreation Facilities Guidelines to include a pool and associated club house and amenities, pet spa, pet wash equipment, dog park, tot lot, and community garden. In addition, the applicant agrees to contribute \$50,000.00 to the City's Parks and Recreation Department for use towards City improvements planned for the Springhill Lake Recreation Center. Timing of said contribution shall be agreed upon prior to the issuance of the first building permit by the City.

7. Prior to the submission of a final record plat, the Applicant agrees to execute a Private Recreation Facilities Agreement with the City to provide for the retention and future maintenance of proposed private recreation facilities. The executed agreement shall be recorded among the land records of Prince Georges County.

ⁱ Primary Management Area - A vegetated buffer preserved and/or restored along all regulated streams outside the Chesapeake Bay Critical Area Overlay Zones, which at a minimum includes:

- (A) All regulated streams and associated minimum stream buffers;
- (B) The one hundred (100) year floodplain as defined by Section 27-124.01;
- (C) All wetlands and associated wetland buffers that are adjacent to the regulated stream, stream buffer or the one hundred (100) year floodplain;
- (D) All areas having slopes of fifteen (15%) or greater adjacent to the regulated stream or stream buffer, the one hundred (100) year floodplain, or adjacent wetlands or wetland buffers;
- (E) Adjacent critical habitat areas

ⁱⁱ STC - Sound Transmission Class

1. SUBDIVISION NAME: GREENBELT METRO
2. PARCEL AREA: 15.89
3. EXISTING ZONE: C-O, D-D-O
4. PROPOSED USE: MULTI-FAMILY WITH AMENITY SPACE
5. LOTS: 0
PARCELS: 1
OUTLOTS: 0
OUTPARCELS: 0
6. DWELLING UNIT TYPES: MULTI-FAMILY
55 - STUDIO
157 - ONE-BEDROOM
123 - TWO-BEDROOM
19 - THREE-BEDROOM
7. GROSS FLOOR AREA: RESIDENTIAL 388,225 SQ.FT.
AMENITY 7,082 SQ.FT.
TOTAL 395,307 SQ.FT.
8. WSSC GRID: 212NE06
9. TAX MAP: 026C2, ACCOUNT #13-136942, #13-136959, #13-136967
10. AVIATION POLICY AREA: NOT WITHIN
11. MILITARY INSTALLATION AREA: NOT WITHIN
12. EXISTING WATER DESIGNATION: W-3
13. EXISTING SEWER DESIGNATION: S-3
14. PROPOSED WATER DESIGNATION: W-3
15. PROPOSED SEWER DESIGNATION: S-3
16. SITE DEVELOPMENT CONCEPT PLAN #59556-2018-O.
17. 10-FOOT PUBLIC UTILITY EASEMENT ALONG ALL RIGHTS-OF-WAY PER ALTA, THE PUE IS EXISTING.
18. MANDATORY PARK DEDICATION: 5% OR FEE-IN-LIEU
19. ARE NOT CEMETERIES EXIST ON OR CONTIGUOUS TO PROPERTY
20. NO HISTORIC SITES ON OR IN THE VICINITY OF THE PROPERTY
21. THE REGULATED 1-PERCENT ANNUAL CHANCE (100-YEAR) FLOODPLAIN INFORMATION ON THIS PLAN, APPROVED BY THE PRINCE GEORGE'S DEPARTMENT OF PERMITTING, INSPECTIONS AND ENFORCEMENT, IS FROM APPROVED CASE NO. 58937-2018 / FPS NO. 201916 DATED APRIL 10, 2019. THE 100-YEAR FLOOD PLAIN ELEVATION IS 81.73' NGVD29.
22. SITE IS NOT WITHIN CHESAPEAKE BAY CRITICAL AREA
23. SOURCE OF TOPOGRAPHY: VIKA MARYLAND, LLC
24. NRP GROUP, LLC
25. 9801 WASHINGTONIAN BLVD., SUITE 310
26. GATHERSBURG, MD 20878
27. 240.862.1489
28. CONTACT: KARL J. ALIT

	HEIGHT
BUILDING 1000	69.17'
BUILDING 2000	69.17'
GARAGE 1000	20.67'
GARAGE 2000	20.67
CLUBHOUSE	22'

	SLAB ELEVATION (A)	HEIGHT (FT) * (B)	STREET GRADE @ CL (C)	BUILDING HEIGHT (A)+(B)-(C)
BUILDING 1000	93.4	62	106.3	49.1
BUILDING 2000	93	62	89.6	65.4

	SLAB ELEVATION (A)	AVG. GRADE (B)	HEIGHT (FT) * (C)	BUILDING HEIGHT (A)-(B)+(C)
GARAGE 1000	92.75	93.25	17	16.5

	HIGHEST SLAB ELEVATION/AVG. GRADE (A)	LOWEST SLAB ELEVATION/AVG. GRADE (B)	HEIGHT (FT) * (C)	BUILDING HEIGHT (A)-(B)+(C)
GARAGE 2000	90.92	90.25	17	17.67

	SLAB ELEVATION (A)	AVG. GRADE (B)	HEIGHT (FT) * (C)	BUILDING HEIGHT (A)-(B)+(C)
CLUBHOUSE	92	91.8	21	21.2

	REQUIRED:	PROVIDED:
FRONT BUILD TO LINE DISTANCE: 1387'	60% OR 832 LF	39.7% OR 552 LF

<u>PERMITTED MAXIMUM:</u>	<u>PROVIDED:</u>
80%	37.5%
12.712 AC OR 553,735 SF	5.96 AC OR 259,617 SF

BUILDING SETBACKS		
FRONT	REQUIRED (PER DDOZ DEVELOPMENT STANDARDS)	PROVIDED
BUILDING 1000:	15'-20'	29.6'
BUILDING 2000:	15'-20'	22.87'

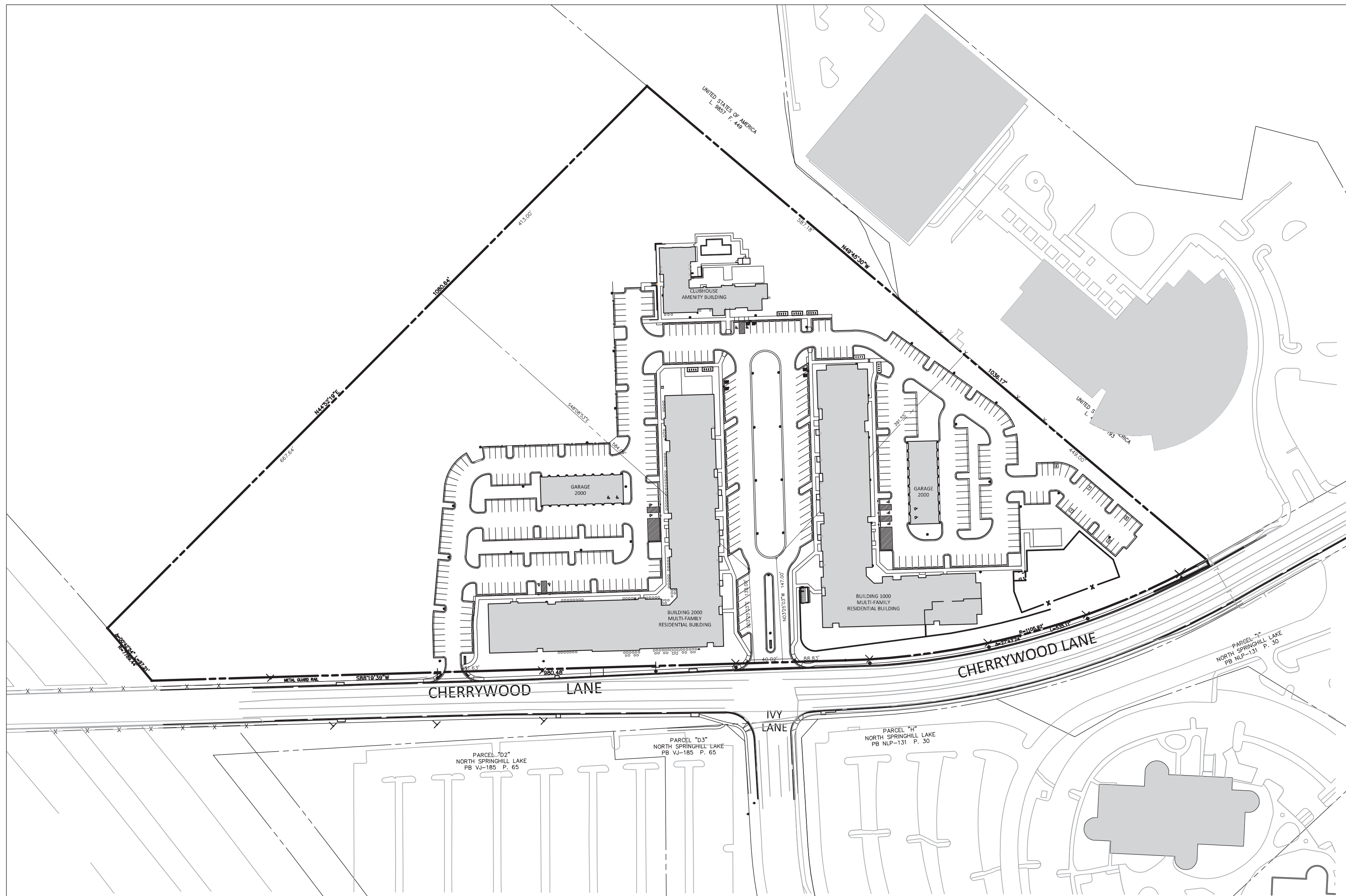
REAR (NW PROPERTY LINE)	REQUIRED (PER ZONING/ LANDSCAPE MANUAL)	PROVIDED
ADJACENT ZONE: R-R USE: VACANT	25' * OR THE BUFFER REQUIRED IN THE LANDSCAPE MANUAL, WHICHEVER IS GREATER	
BUILDING 2000:	25' + (65.4/3) = 46.8'	± 301.3' MIN
GARAGE 2000:	0'	± 258.0' MIN
CLUBHOUSE:	0'	± 148.5' MIN

BUILDING 1000:	30' (BUFFER TYPE B)	± 73.8' MIN.
GARAGE 1000:	0'	± 88.0' MIN.
CLUBHOUSE:	0'	± 73.3' MIN.

DETAILED SITE PLAN

GREENBELT METRO

DSP-19014/00



PARKING			
UNIT TYPE	# OF UNITS	RATE PER SEC. 27-568	REQUIRED PER SEC. 27-568
1 BED (INCLUDES STUDIO)	212	1.33	282
2 BED	123	1.66	205
3 BED	19	1.99	38
TOTAL	354		525
DDOZ MINIMUM PARKING REQUIRED: (70% OF STANDARD REQUIREMENT):			368
DDOZ MAXIMUM PARKING PERMITTED: (80% OF STANDARD REQUIREMENT)			420
COMPACT PERMITTED: (1/3 OF WITHOUT DEPARTURE)			
PARKING PROVIDED:			
GARAGE	ACCESSIBLE SPACES:		4 SPACES
	GARAGE SPACES:		32 SPACES
	TOTAL:		36 SPACES
SURFACE	ACCESSIBLE SPACES:		9 SPACES
	STANDARD SPACES (9'X18'):		285 SPACES
	COMPACT SPACES (8'X16.5'):		124 SPACES
	TOTAL:		418 SPACES
	GRAND TOTAL*:		454 SPACES
* INCLUDES 6 ELECTRIC VEHICLE CHARGING SPACES OF WHICH 1 IS ACCESSIBLE.			
	# OF UNITS	PARKING RATE	REQUIRED PROVIDED
DDOZ BIKE PARKING:	354	0.5	177 SPACES 136 SPACES
			80 INTERIOR SPACES 56 EXTERIOR SPACES
LOADING			
USE TYPE	# OF UNITS	RATE PER SEC. 27-582	REQUIRED PER SEC. 27-582
MULTIFAMILY DWELLING	354	1 FOR FIRST 300 1 FOR EVERY ADDITIONAL 200	1 1
LOADING PROVIDED:			2
	BUILDING 1000		1 SPACES
	BUILDING 2000		1 SPACES
	TOTAL:		2 SPACES

[illegible]

VICINITY MAP
SCALE: 1" = 2000'

VIKA

ENGINEERS PLANNERS
LANDSCAPE ARCHITECTS SURVEYORS
VIKA MARYLAND, LLC
20251 CENTURY BOULEVARD SUITE #400
GERMANTOWN, MARYLAND 20874
PHONE: (301) 916-4100
FAX: (301) 916-2262
GERMANTOWN, MD. TYSONS, VA.

PREPARED FOR:
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KARL ALT

CIVIL ENGINEER
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MATTHEW TEDESCO

645 BALTIMORE BOULEVARD
SUITE 214
SEVERNA PARK, MD. 21146
410.216.333
MIKE LENHART

DSP-1	COVER SHEET
DSP-2	PLAN APPROVALS
DSP-3	EXISTING CONDITIONS
DSP-4	COMPOSITE SITE PLAN
DSP-5	SITE PLAN BUILDING 1
DSP-6	SITE PLAN BUILDING 2
DSP-7	SITE PLAN COMMUNITY BUILDING
DSP-8	SITE DETAILS
DSP-9	SITE DETAILS

DSP-10	LANDSCAPE AND LIGHTING COVER SHEET
DSP-11	LANDSCAPE COMPUTATIONS
DSP-12	LANDSCAPE PLAN
DSP-13	LANDSCAPE PLAN
DSP-14	LANDSCAPE PLAN
DSP-15	LANDSCAPE PLAN
DSP-16	LANDSCAPE PLAN
DSP-17	CLUBHOUSE DETAIL PLAN
DSP-18	PLANT LIST & LANDSCAPE DETAILS
DSP-19	HARDSCAPE DETAILS
DSP-20	HARDSCAPE DETAILS
DSP-21	CLUBHOUSE FURNITURE
DSP-22	ENTRANCE MONUMENT DETAILS
DSP-23	OVERALL SWM LANDSCAPE PLAN
DSP-24	SWM LANDSCAPE PLANS
DSP-25	SWM LANDSCAPE PLANS
DSP-26	SWM LANDSCAPE PLANS
DSP-27	LIGHTING PLAN
DSP-28	LIGHTING PLAN
DSP-29	LIGHTING PLAN
DSP-29.1	LIGHTING PLAN
DSP-29.2	LIGHTING PLAN

DSP-31	BUILDING 1000 ELEVATIONS
DSP-32	BUILDING 2000 ELEVATIONS
DSP-33	GARAGE ELEVATIONS
DSP-34	CLUBHOUSE ELEVATIONS

[illegible]

I HEREBY CERTIFY THAT THESE DOCUMENTS WERE
PREPARED OR APPROVED BY ME, AND THAT I AM A
DULY LICENSED PROFESSIONAL ENGINEER UNDER
THE LAWS OF THE STATE OF MARYLAND.
DONALD J. NELSON LICENSE No. 52939
EXPIRATION: JULY 11, 2020

GREENBELT
METRO

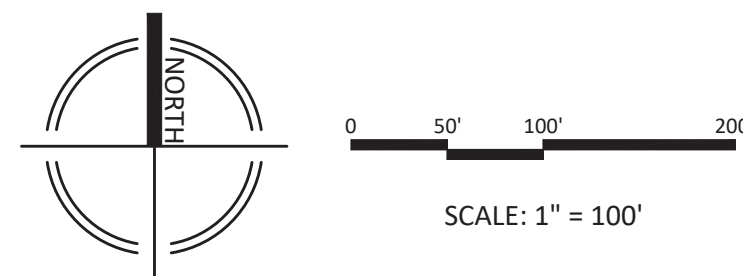
21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

COVER
SHEET

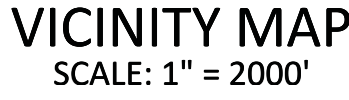
DRAWN BY: JB/CC
DESIGNED BY: JB/DN
DATE ISSUED: 6/7/19

VIKA
PROJECT VM50254C
DRAWING
NO. C-201

SHEET NO. DSP-1



FOR OFFICIAL USE ONLY QR label certifies that this plan meets conditions of final approval by the Planning Board, its designee or the District Council. M-NCPPC APPROVAL	
PROJECT NAME: GREENBELT METRO	
PROJECT NUMBER: DSP-19014/00	
For Conditions of Approval see Site Plan Cover Sheet or Approval Sheet Revision numbers must be included in the Project Number	



PREPARED FOR:
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MIKE LENHART

[illegible]

I HEREBY CERTIFY THAT THESE DOCUMENTS WERE PREPARED OR APPROVED BY ME, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MARYLAND.
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EXPIRATION: JULY 11, 2020

**GREENBELT
METRO**
21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

PLAN APPROVALS

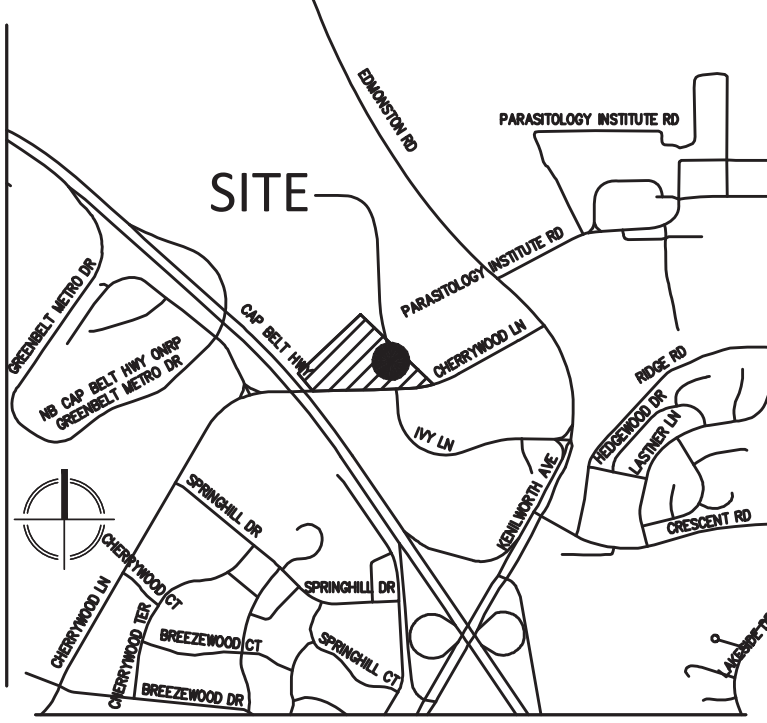
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DRAWN BY: JB/CC
DESIGNED BY: JB/DN
DATE ISSUED: 6/7/19

VIKA
PROJECT **VM50254C**

DRAWING
NO. **C-202**

SHEET NO. **DSP-2**



VICINITY MAP
SCALE: 1" = 2000'



PREPARED FOR:
THE NRP GROUP, LLC
1228 EUCLID AVENUE
4TH FLOOR
CLEVELAND, OH. 44115
216.584.0602
KARL ALT

DESIGN CONSULTANTS

CIVIL ENGINEER
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8200 GREENSBORO DRIVE
SUITE 650
MCLEAN, VA. 22102
571.253.6950
DON FOOR III, RA

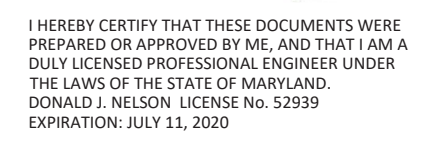
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703.642.2306
ADAM STEINER, PLA

ATTORNEY
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LENHART TRAFFIC CONSULTING, INC.
645 BALTIMORE BOULEVARD
SUITE 214
SEVERNA PARK, MD. 21146
410.216.333
MIKE LENHART

[illegible]

PROFESSIONAL SEAL



GREENBELT
METRO

21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

EXISTING
CONDITIONS
PLAN

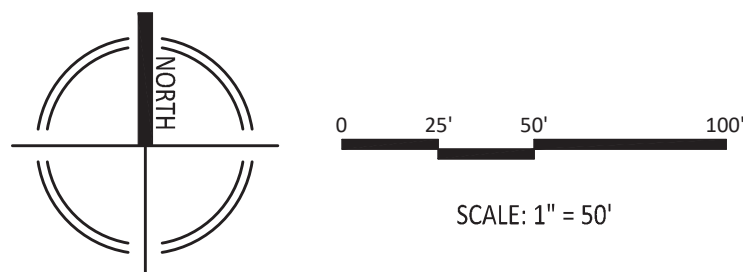
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PROJECT NAME:	GREENBELT METRO
PROJECT NUMBER:	DSP-19014/00
<i>For Conditions of Approval see Site Plan Cover Sheet or Approval Sheet</i> <i>Revision numbers must be included in the Project Number</i>	

DRAWN BY: JB/CC
DESIGNED BY: JB/DN
DATE ISSUED: 6/7/19

VIKA
PROJECT **VM50254C**

DRAWING
NO. **C-203**

SHEET NO. **DSP-3**





**"FOR LOCATION OF UTILITIES CALL
8-1-1 or 1-800-257-7777 OR LOG ON TO
www.kc811.com or <http://www.missutility.net>
48 HOURS IN ADVANCE OF ANY
WORK IN THIS VICINITY"**

The excavator must notify all public utility companies with underground facilities in the area of proposed excavation and have those facilities located by the utility companies prior to commencing excavation. The excavator is responsible for compliance with requirements of Chapter 36A of the Montgomery County Code.

[illegible]

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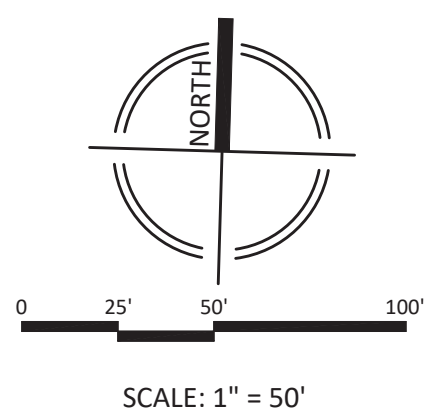
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410.216.333
MIKE LENHART

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THE LAWS OF THE STATE OF MARYLAND.
DONALD J. NELSON LICENSE NO. 52939
EXPIRATION: JULY 11, 2020

21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

COMPOSITE
DETAILED
SITE
PLAN



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PROJECT NUMBER:	DSP-19014/00
For Confidentiality of Approval see Site Plan Cover Sheet or Approval Sheet Revision numbers must be included in the Project Number	

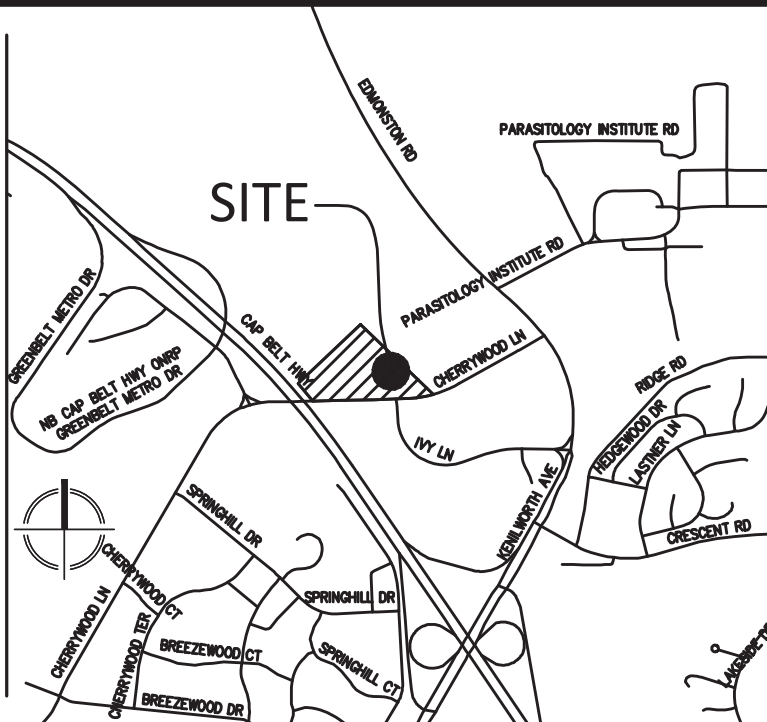
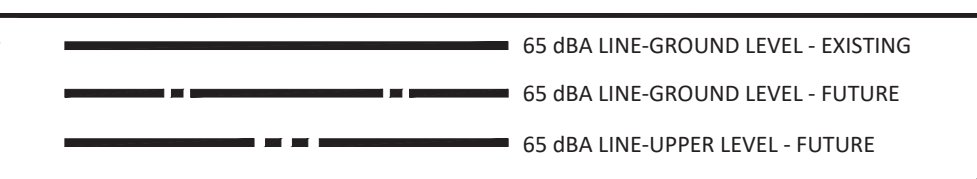
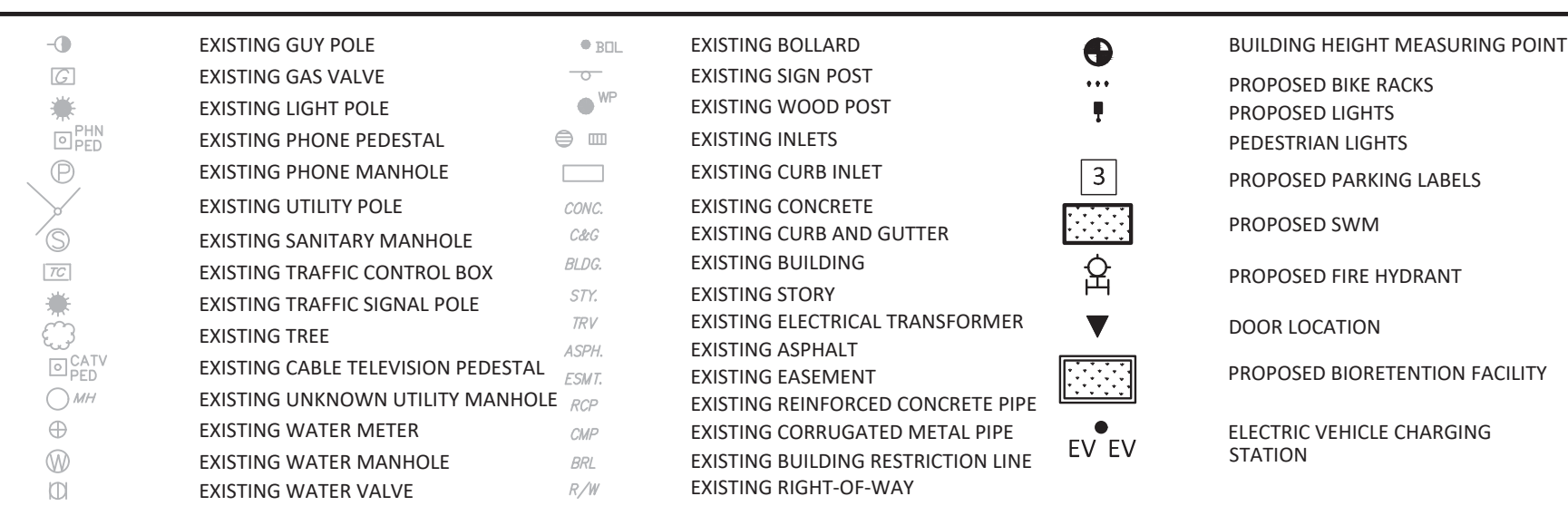
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VIKA
PROJECT **VM50254C**

DRAWING
NO. **C-204**

SHEET NO. DSP-4

PROPERTY LINES		PROPOSED WATER LINE	
EXISTING CABLE TELEVISION CONDUIT		PROPOSED SANITARY SEWER WITH STRUCTURE	
EXISTING ELECTRICAL CONDUIT		PROPOSED STORM DRAIN	
EXISTING EDGE OF PAVEMENT		PROPOSED LIMITS OF DISTURBANCE	
EXISTING FENCE LINE		PROPOSED STORM WATER EASEMENT	
EXISTING NATURAL GAS CONDUIT		EXISTING PARKING LABEL	
EXISTING OVERHEAD WIRES		EXISTING SANITARY CLEANOUT	
EXISTING TELEPHONE CONDUIT		EXISTING STORM DRAIN MANHOLE	
EXISTING PUBLIC UTILITIES EASEMENTS		EXISTING ELECTRICAL JUNCTION BOX	
EXISTING SANITARY SEWER CONDUIT		EXISTING ELECTRICAL MANHOLE	
EXISTING STORM DRAIN CONDUIT		EXISTING FIRE DEPARTMENT CONNECTION	
EXISTING WATER CONDUIT		EXISTING FIRE HYDRANT	
EXISTING ZONE LIMITS		EXISTING GAS MANHOLE	
PROPOSED 10' CONTOUR			
PROPOSED 2' CONTOUR			



VICINITY MAP
SCALE: 1" = 2000'



PREPARED FOR:
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DONALD J. NELSON LICENSE No. 52939
EXPIRATION: JULY 11, 2020

21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

DETAILED
SITE
PLAN

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PROJECT NAME: GREENBELT METRO	
PROJECT NUMBER: DSP-19014/00	
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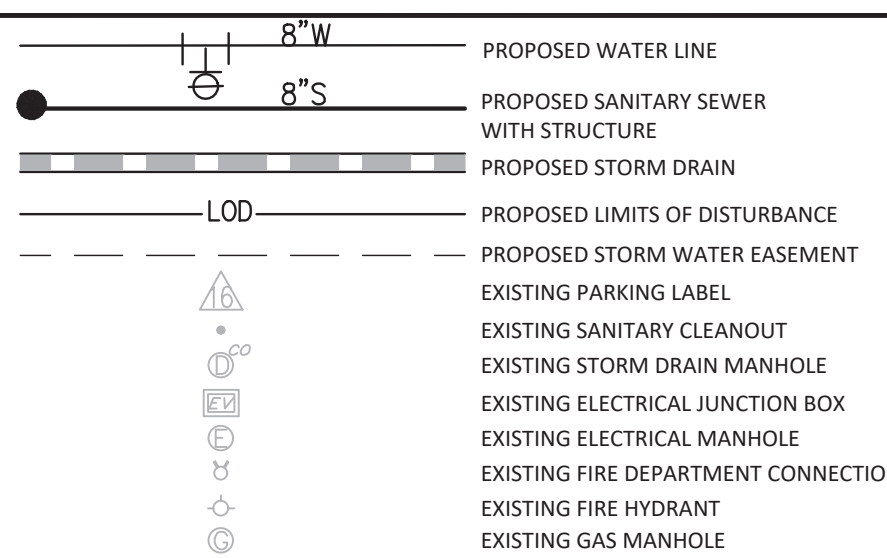
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DATE ISSUED: 6/7/19

VIKA
PROJECT **VM50254C**

DRAWING
NO. **C-205**

DSD E

	PROPERTY LINES
	EXISTING CABLE TELEVISION CONDUIT
	EXISTING ELECTRICAL CONDUIT
	EXISTING EDGE OF PAVEMENT
	EXISTING FENCE LINE
	EXISTING NATURAL GAS CONDUIT
	EXISTING OVERHEAD WIRES
	EXISTING TELEPHONE CONDUIT
	EXISTING PUBLIC UTILITIES EASEMENT
	EXISTING SANITARY SEWER CONDUIT
	EXISTING STORM DRAIN CONDUIT
	EXISTING WATER CONDUIT
	EXISTING ZONE LIMITS
	PROPOSED 10' CONTOUR
	PROPOSED 2' CONTOUR



EXISTING GUY POLE	9 IC
EXISTING GAS VALVE	" "
EXISTING LIGHT POLE	●
EXISTING PHONE PEDESTAL	Ⓜ
EXISTING PHONE MANHOLE	□
EXISTING UTILITY POLE	CNC.
EXISTING SANITARY MANHOLE	CONC.
EXISTING TRAFFIC CONTROL BOX	BUDG.
EXISTING TRAFFIC SIGNAL POLE	STY.
EXISTING TREE	TRE.
EXISTING CABLE TELEVISION PEDESTAL	ASD.
EXISTING UNKNOWN UTILITY MANHOLE	EQUIP.
EXISTING WATER METER	RMP.
EXISTING WATER MANHOLE	WMP.
EXISTING WATER VALVE	R/W.

EXISTING BOLLARD
EXISTING SIGN POST
EXISTING WOOD POST
EXISTING INLETS
EXISTING CURB INLET
EXISTING CONCRETE
EXISTING CURB AND GUTTER
EXISTING BUILDING
EXISTING STORY
EXISTING ELECTRICAL TRANSFORMER
EXISTING ASPHALT
EXISTING EASEMENT
EXISTING REINFORCED CONCRETE PIPE
EXISTING CORRUGATED METAL PIPE
EXISTING BUILDING RESTRICTION LINE
EXISTING RIGHT-OF-WAY

	BUILDING HEIGHT MEASURING POINT
	PROPOSED BIKE RACKS
	PROPOSED LIGHTS
	PEDESTRIAN LIGHTS
	PROPOSED PARKING LABELS
	PROPOSED SWM
	PROPOSED FIRE HYDRANT
	DOOR LOCATION
	PROPOSED BIORETENTION FACILITY
	ELECTRIC VEHICLE CHARGING STATION

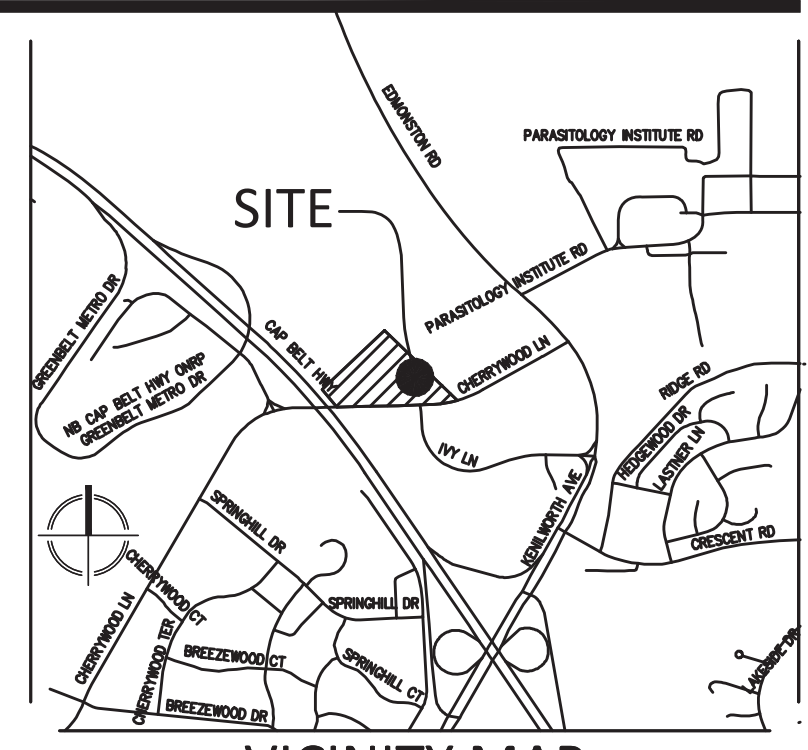
T

65 GBA LINE-GROUND LEVEL - EXISTING

65 GBA LINE-GROUND LEVEL - FUTURE

65 GBA LINE-UPPER LEVEL - FUTURE

1



SCALE: 1" = 2000'



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FAX: (301) 916-2262
GERMANTOWN, MD. TYSONS, VA.

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KARL ALT

DESIGN CONSULTANT:

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MIKE LENHART

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DONALD J. NELSON LICENSE No. 52939
EXPIRATION: JULY 11, 2020

**GREENBELT
METRO**

21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

DETAILED SITE PLAN

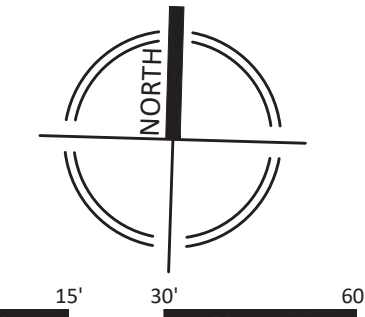
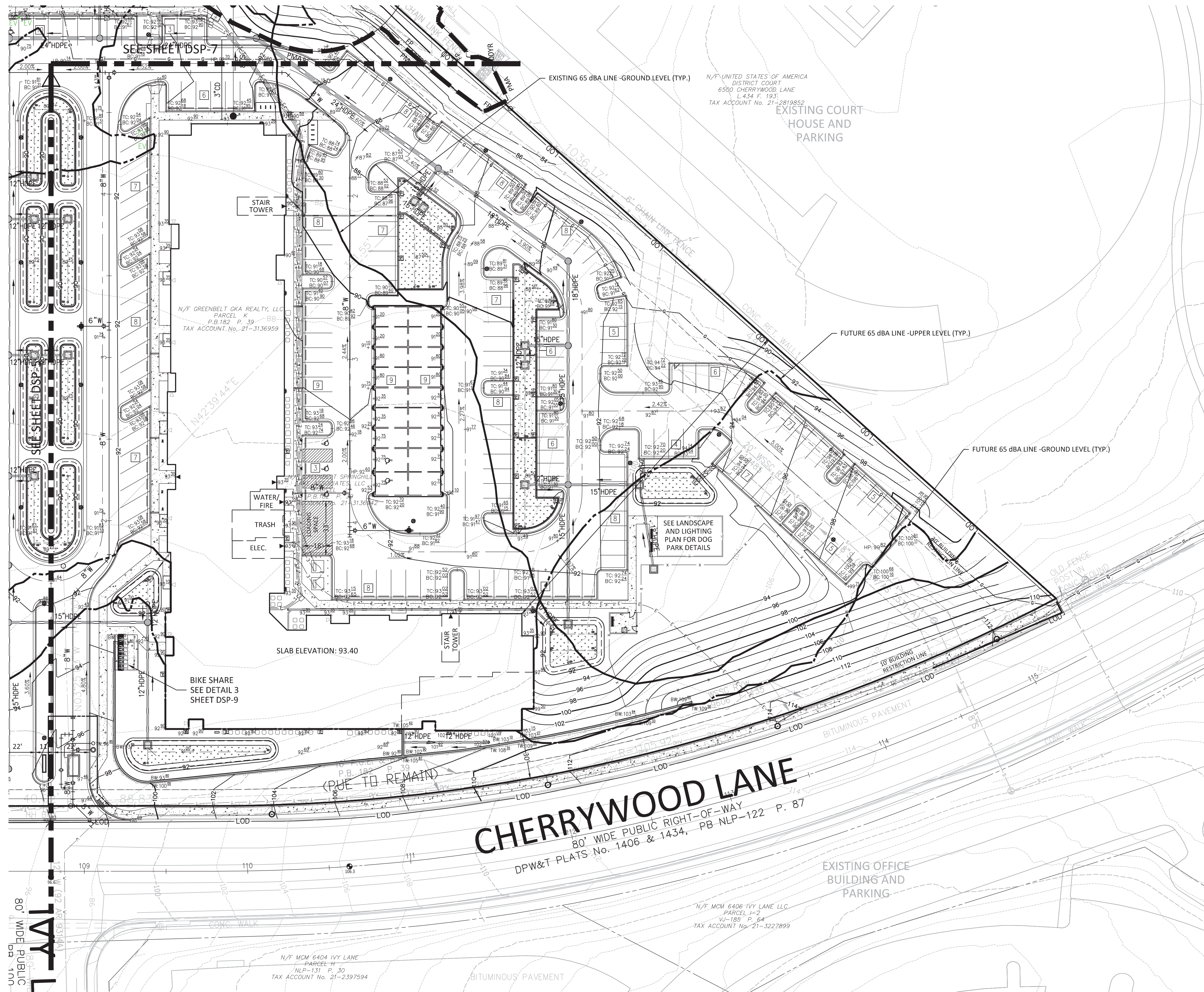
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DRAWN BY: JB/CC
DESIGNED BY: JB/DN
DATE ISSUED: 6/7/19

VIKA
PROJECT **VM50254C**

DRAWING
NO. **C-206**

SHEET NO. DSP-6



SCALE: 1" = 30'

[illegible]

PREPARED FOR:
THE NRP GROUP, LLC
1228 EUCLID AVENUE
4TH FLOOR
CLEVELAND, OH. 44115
216.584.0602
KARL ALT

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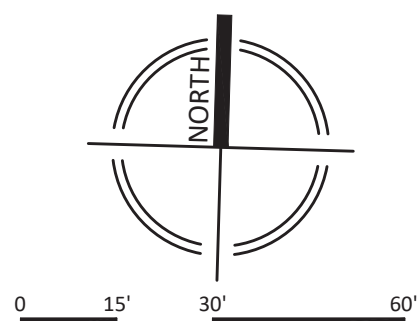
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DONALD J. NELSON LICENSE No. 52939
EXPIRATION: JULY 11, 2020

**GREENBELT
METRO**
21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

DETAILED
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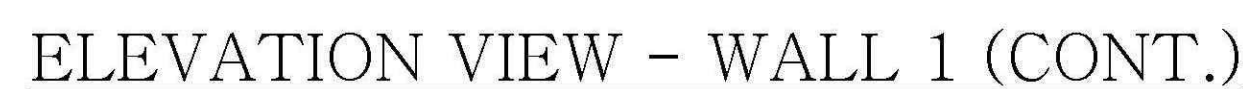
VIKA
PROJECT **VM50254C**

DRAWING
NO. **C-207**

SHEET NO. DSP-7



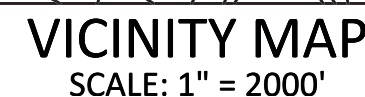
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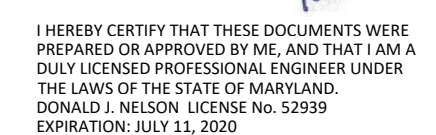
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[illegible]

PROFESSIONAL SEAL



**GREENBELT
METRO**
21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

SITE DETAILS

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VIKA
PROJECT VM50254C
DRAWING
NO. C-208

SHEET NO. DSP-8



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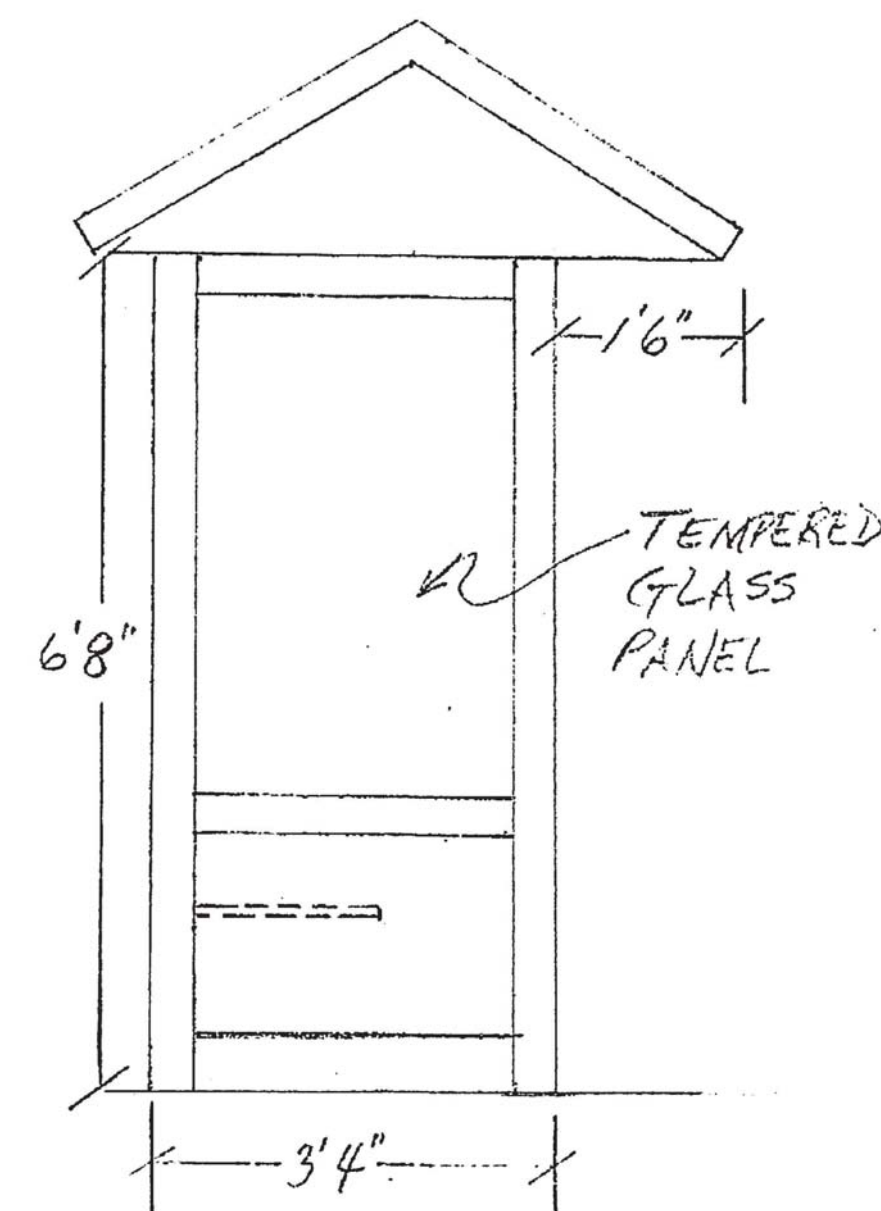


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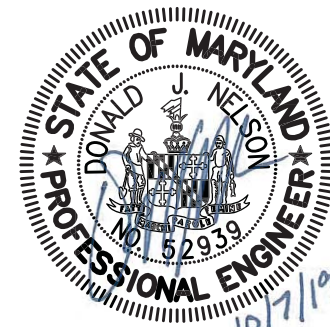
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[illegible]

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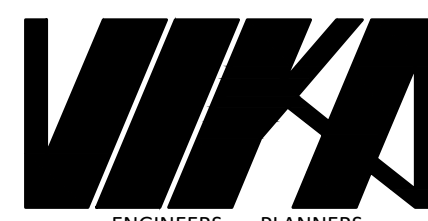
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DRAWN BY:	<u>JB/CC</u>
DESIGNED BY:	<u>JB/DN</u>
DATE ISSUED:	<u>6/7/19</u>
VIKA PROJECT	VM50254C
DRAWING NO.	C-209
SHEET NO.	DSP-9



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FAX: (301) 916-2262
GERMANTOWN, MD. TYSONS, VA.

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CLEVELAND, OH. 44115
216.584.0602
KARL ALT

DESIGN CONSULTANTS

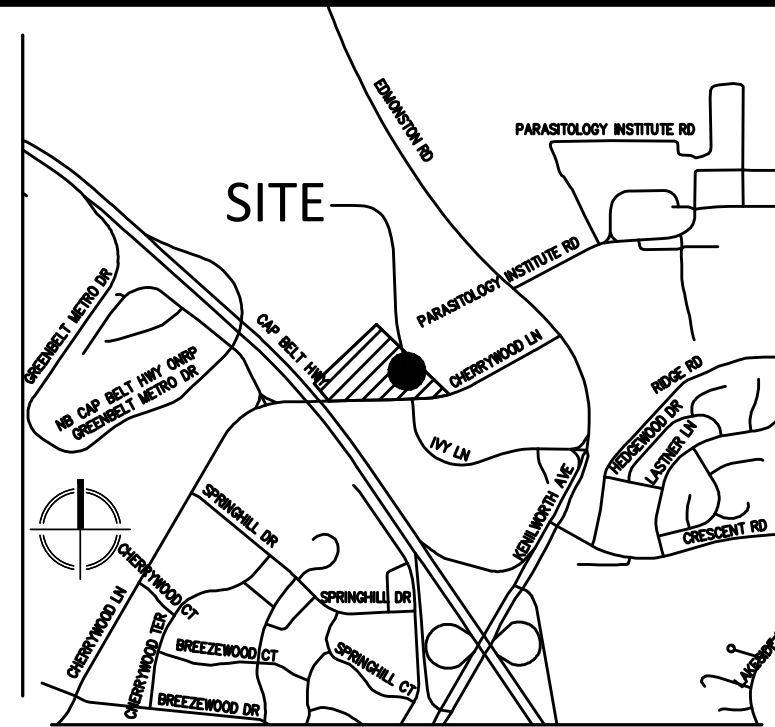
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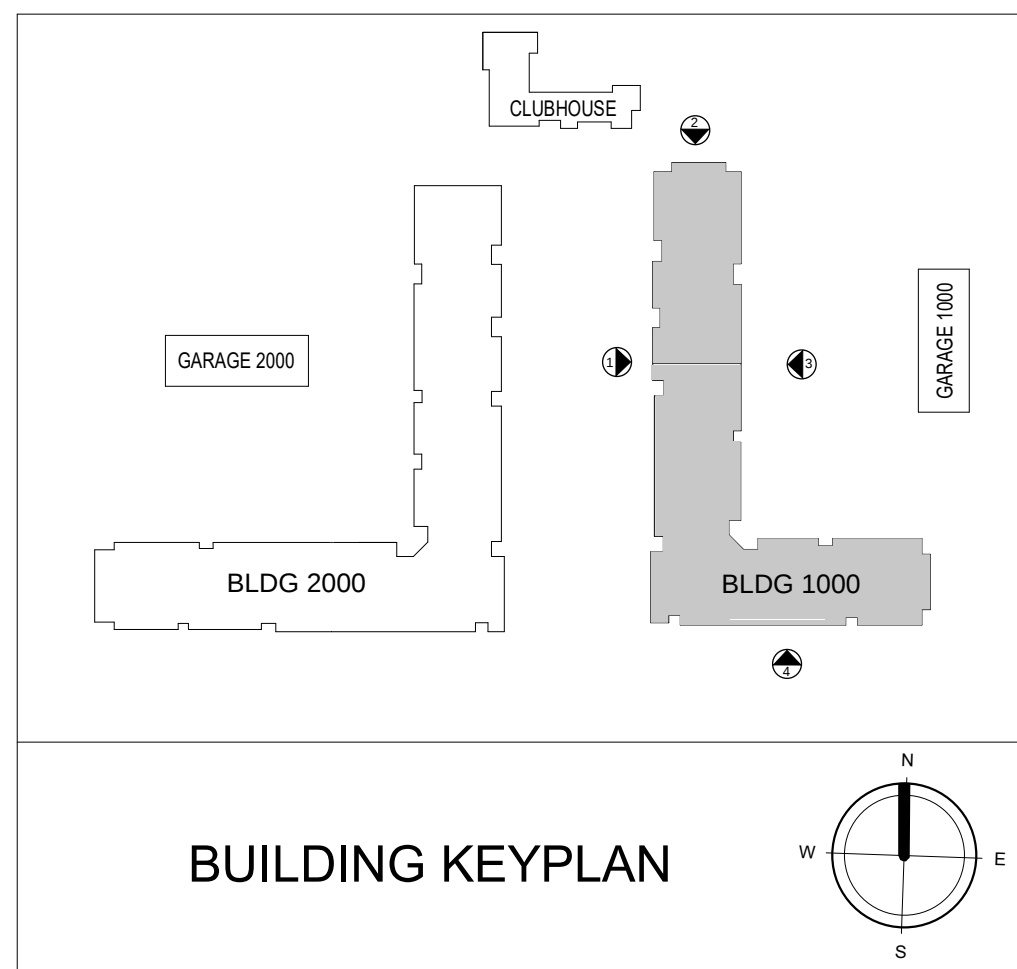
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ADAM STEINER, PLA

ATTORNEY
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GREENBELT, MD. 20770
301.441.2420
MATTHEW TEDESCO

TRAFFIC ENGINEER
LENHART TRAFFIC CONSULTING, INC
645 BALTIMORE BOULEVARD
SUITE 214
SEVERNA PARK, MD. 21146
410.216.333
MIKE LENHART



VICINITY MAP
SCALE: 1" = 2000'



BUILDING KEYPLAN

EXTERIOR FINISH LEGEND

1.1	BRICK VENEER
1.2	PAINTED CONCRETE
2.1	CEMENTITIOUS PANEL WITH REVEAL 1
2.2	CEMENTITIOUS PANEL WITH REVEAL 2
2.3	CEMENTITIOUS PANEL WITH REVEAL 3
3.1	CEMENTITIOUS SIDING 1
3.2	CEMENTITIOUS SIDING 2
3.3	CEMENTITIOUS SIDING 3
3.4	CEMENTITIOUS SIDING 4
4.1	METAL PANEL 1
4.2	METAL PANEL 2
5.1	METAL BALCONY RAILING
5.2	STOREFRONT SYSTEM
5.3	ARCHITECTURAL SIGNAGE
5.4	METAL CANOPY
6.1	ASPHALT SHINGLE ROOF

REVISIONS	DATE

PROFESSIONAL SEAL

GREENBELT METRO
21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

BLDG 1000 ELEVATIONS

DRAWN BY: _____
DESIGNED BY: _____
DATE ISSUED: _____
VIKA PROJECT _____
DRAWING NO. _____
SHEET NO. DSP-31

FOR OFFICIAL USE ONLY
Official certifies that this plan meets conditions of final approval by the Planning Board, its designee or the District Council.
M-NCPCC APPROVAL
PROJECT NAME: GREENBELT METRO
PROJECT NUMBER: DSP-19014/00
For Conditions of Approval see Site Plan Cover Sheet or Approval Sheet
Revision numbers must be included in the Project Number



4 BUILDING 1000 - SOUTH ELEVATION
1/16" = 1'-0"



3 BUILDING 1000 - EAST ELEVATION
1/16" = 1'-0"



2 BUILDING 1000 - NORTH ELEVATION
1/16" = 1'-0"



1 BUILDING 1000 - WEST ELEVATION
1/16" = 1'-0"



1.2	PAINTED CONCRETE
3.3	CEMENTITIOUS SIDING 3
3.5	CEMENTITIOUS SIDING 5
6.1	ASPHALT SHINGLE ROOF

[illegible]

PROFESSIONAL SEAL

GREENBELT
METRO

21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

GARAGE ELEVATIONS

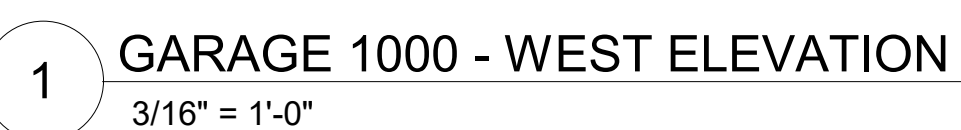
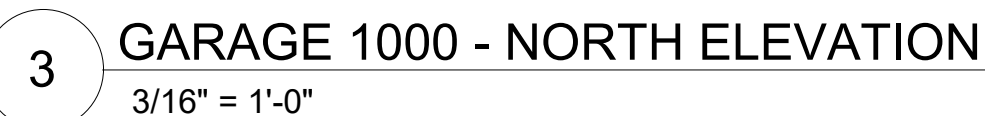
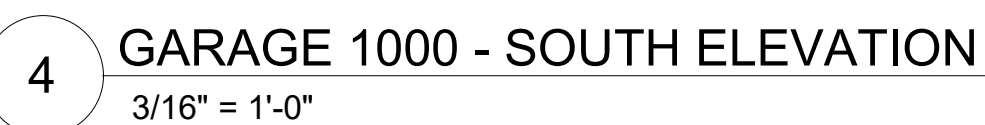
FOR OFFICIAL USE ONLY QR label certifies that this plan meets conditions of final approval by the Planning Board, its designee or the District Council. M-NCPPC APPROVAL	
PROJECT NAME: GREENBELT METRO	
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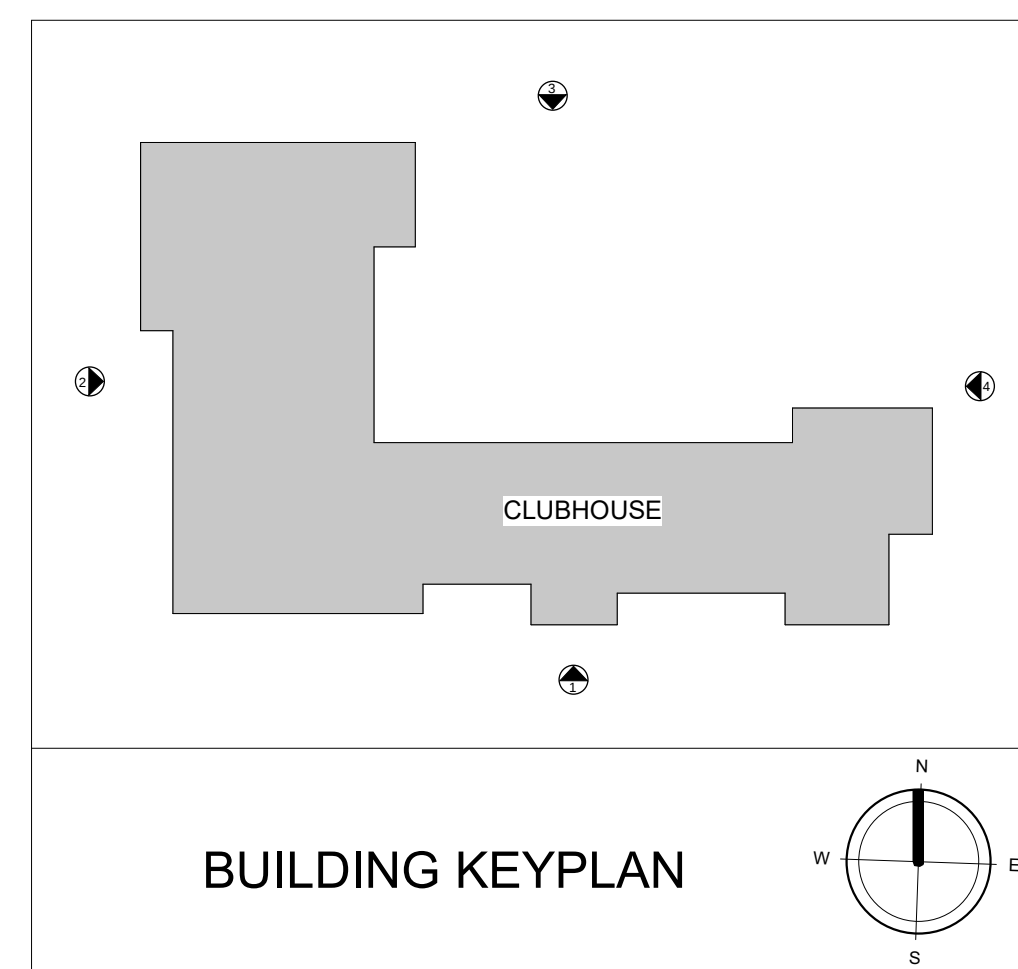
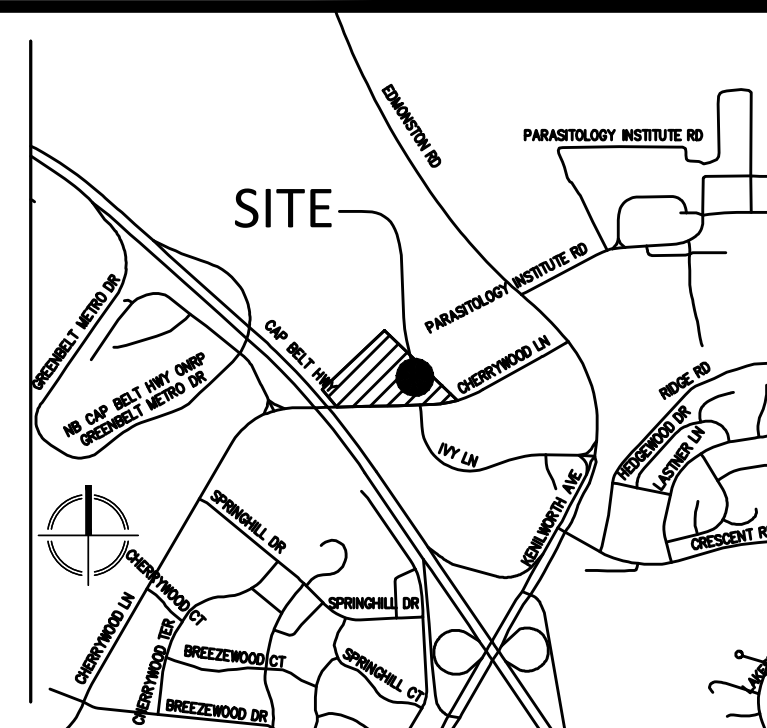
DRAWN BY: _____
DESIGNED BY: _____
DATE ISSUED: _____

VIKA
PROJECT

DRAWING
NO.

SHEET NO. DSP-33





EXTERIOR FINISH LEGEND	
1.3	GROUND FACE CMU
2.2	CEMENTITIOUS PANEL WITH REVEAL 2
2.3	CEMENTITIOUS PANEL WITH REVEAL 3
2.4	CEMENTITIOUS PANEL WITH REVEAL 4
2.5	CEMENTITIOUS PANEL WITH REVEAL 5
3.3	CEMENTITIOUS SIDING 3
5.4	METAL CANOPY
6.1	ASPHALT SHINGLE ROOF

[illegible]

PROFESSIONAL SEAL

GREENBELT
METRO

21ST ELECTION DISTRICT
PRINCE GEORGE'S COUNTY,
MARYLAND
WSSC GRID: 212NE06
TAX MAP: 26-C2
MNCPPC
DSP-19014/00

CLUBHOUSE ELEVATIONS

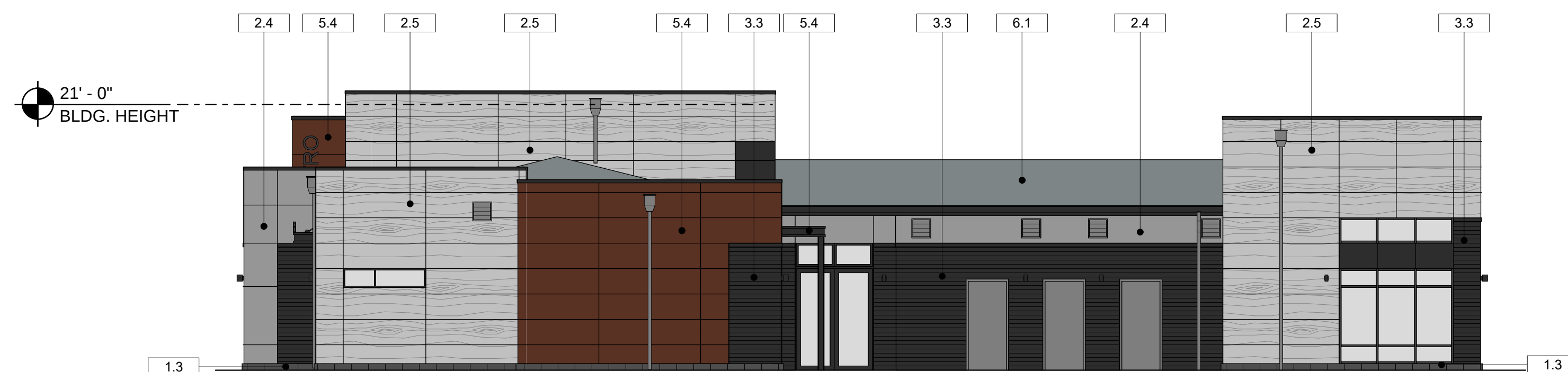
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PROJECT NAME: GREENBELT METRO	
PROJECT NUMBER: DSP-19014/00	
<i>For Certificates of Approval see Site Plan Cover Sheet or Approval Sheet</i> <i>Revision numbers must be included in the Project Number</i>	

DRAWN BY: _____
DESIGNED BY: _____
DATE ISSUED: _____

VIKA
PROJECT

DRAWING
NO.

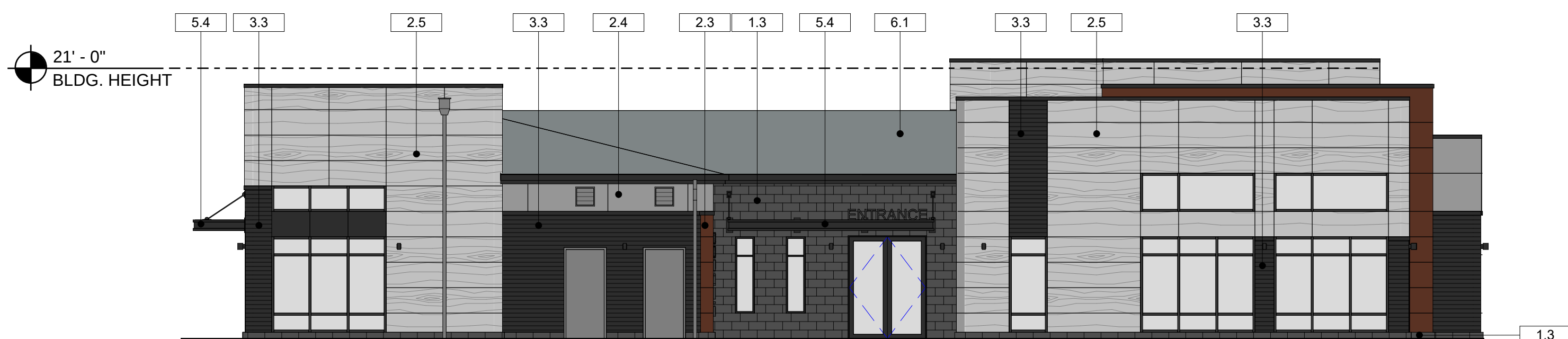
SHEET NO. DSP-34



4 EAST ELEVATION
1/8" = 1'-0"



3 NORTH ELEVATION
1/8" = 1'-0"



2 WEST ELEVATION
1/8" = 1'-0"



1 SOUTH ELEVATION
1/8" = 1'-0"

**PARK AND RECREATION ADVISORY BOARD
REPORT TO CITY COUNCIL**

Subject: Capital Office Park Detailed Site Plan

Background: PRAB, with APB joining for the presentation and discussion, met with representatives from NRP and related business partners on November 20, 2019 to discuss the Detailed Site Plan for the Capital Office Park. The presentation included a description of the plan, modifications to the plan based on other meetings with Council and other advisory boards, and was followed by a series of questions and comments by PRAB and APB.

Recommendations: PRAB understands that the detailed site plan is a general guide and is not a final, approved construction plan. We also feel that the group took into consideration our requests and recommendations from previous meetings. PRAB would like to provide some feedback and requests as the process moves forward, in addition to previously stated recommendations:

- Collaborate and cooperate with the Recreation Department. This increase in residency will only exacerbate the need for recreation programming in the area. Consider a partnership for public use of private facilities (fitness center, playground, dog park, etc.).
- Consider the use and utilization of the indoor programming space in the fitness facility, encouraging classes for a variety of ages and activity levels, and in a variety of formats.
- Include additional, passive recreation space, such as a pervious surface walking/jogging trail around the property perimeter.

PRAB recognizes and appreciates the proposed \$50,000 donation to Springhill Lake Recreation Center, and any additional funding to assist the Recreation Department will be well-used. To attempt to put this level of funding in perspective: Recreation would be able to use these funds to refurbish the outdoor basketball court, but it will not be, in and of itself, sufficient to address the larger, more complicated and costly concern of limited indoor and outdoor programming space.

More generally, PRAB recommends that Council reevaluates the level of service provided to this part of Greenbelt. We would reiterate the concern that the development of this property, in conjunction with the proposed Beltway Plaza redevelopment project, will not meet the recreation needs of Greenbelt residents on the west side of the city. Springhill Lake Recreation Center does not currently have adequate program space for the existing population, and new residents in the vicinity will only increase demand. PRAB continues to see the expansion of public programming and recreation space in the area as vital to meeting the needs of Greenbelt residents.

Respectfully submitted by:
Jacob Chesnutt, PRAB Chair
Ric Gordon, PRAB Vice-Chair